



Planning Commission
REGULAR MEETING AGENDA
Tuesday, October 28, 2025 | 7:00 PM
City Hall Council Chamber – 70 N. First Street

CALL TO ORDER / ROLL CALL

This Planning Commission meeting will be conducted in person and virtually via video teleconferencing (Zoom) in compliance with the provisions of the Brown Act. A quorum of the Planning Commission will be in attendance at the city of Campbell City Hall. Members of the public may attend this meeting in person at Campbell City Hall or virtually via Zoom at <https://campbellca.gov/PCSignup>. The meeting will also be live streamed on Channel 26, the City's website, and on YouTube at <https://www.youtube.com/@CityofCampbell>.

Written correspondence will be accepted via email at planning@campbellca.gov until 12:00 PM on the day of the meeting, and thereafter may be delivered in-person at the public hearing. Written correspondence will be posted to the City's website and distributed to the Planning Commission. If you choose to email your comments, please indicate in the subject line "FOR PUBLIC COMMENT" and indicate the item number.

Members of the public may be allotted up to two (2) minutes to comment on any public hearing or study session item. Applicants/Appellants and their representatives may be allotted up to a total of five (5) minutes for opening statements and up to a total of three (3) minutes maximum for closing statements. Items requested/recommended for continuance are subject to Planning Commission's consent at the meeting.

APPROVAL OF MINUTES

1. **Approval of Minutes of September 23, 2025 (Roll Call Vote)**

Recommended Action: That the Planning Commission approve the regular meeting minutes of September 23, 2025.

COMMUNICATIONS

AGENDA MODIFICATIONS OR POSTPONEMENTS

ORAL COMMUNICATIONS

This portion of the meeting is reserved for individuals wishing to address the Planning Commission on matters of community concern that are not listed on the agenda. In the interest of time, the Chair may limit speakers to five minutes. Please be aware that State law prohibits the Commission from acting on non-agendized items, however, the Chair may refer matters to staff for follow-up.

PUBLIC HEARING**2. 655 Creekside Way – Site and Architectural Review Permit (Resolution/Roll Call Vote)**

Public Hearing to consider the application of Scout Services to allow alterations to the Courtyard Marriott 162-room hotel, including reconstruction of an existing porte-cochere from a hipped roof to a flat-roof design, minor surface improvements, and a new building color scheme, on property located at **655 Creekside Way**, and superseding the property's prior Planned Development Permit (PLN2006-179). The application under consideration is a Site and Architectural Review Permit. File No.: PLN-2025-112. Staff is recommending that this item be deemed Categorical Exempt from CEQA. Planning Commission action is final unless appealed in writing to the City Clerk within 10 calendar days. Project Planner: *Daniel Fama, Senior Planner*

Recommended Action: Adopt a Resolution approving a Site and Architectural Review Permit.

3. 331 West Hacienda Avenue – Conditional Use Permit (Resolution/Roll Call Vote)

Public Hearing to consider the request of Hamid Shirvani to legalize dancing and live entertainment, limited to karaoke, live bands, and disc jockey (DJ) performances, conducted in association with an existing restaurant and bar (dba Effie's) with nonconforming onsite alcohol sales, and to allow ongoing "late night" business hours (12:30 AM closing on Wednesdays and Thursdays; 1:30 AM closing on Fridays and Saturdays), on property located at **331 W. Hacienda Avenue**. The application under consideration is a Conditional Use Permit. File No.: PLN-2025-111. Staff is recommending that this item be deemed Categorical Exempt from CEQA. Planning Commission action is final unless appealed in writing to the City Clerk within 10 calendar days. Project Planner: *Daniel Fama, Senior Planner*.

Recommended Action: Adopt a Resolution approving a Conditional Use Permit.

4. Inclusionary Housing / Overlay District and Density Bonus Ordinances (Resolution/Roll Call Vote)

Public Hearing to consider recommendation of (1) an ordinance amending Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts) to facilitate housing development and clarify related regulatory requirements; and (2) an ordinance amending Chapter 21.20 (Density Bonuses and Other Housing Incentives) to implement Assembly Bill 1287 (2023) and establish a local density bonus incentive to encourage the production of small-scale "affordable by design" housing units in furtherance with the 2023–2031 Housing Element. File No.: PLN-2025-135. Project Managers: *Eloiza Murillo-Garcia, Housing Manager* and *Stephen Rose and Daniel Fama, Senior Planners*

Recommended Action: Adopt a Resolution recommending that the City Council adopt (1) an ordinance amending Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts) and (2) an ordinance amending Chapter 21.20 (Density Bonuses and Other Housing Incentives).

STUDY SESSION**5. Transit-Oriented Communities (TOC) Policy**

Study Session on Metropolitan Transportation Commission (MTC) Transit-Oriented Communities (TOC) Policy.

Recommended Action: That the Planning Commission receive the report and recommend to the City Council appropriate next steps regarding MTC's Transit-Oriented Communities (TOC) policies, including, but not limited to, supporting their future adoption, recommending amendments to their scope or applicability, or taking no further action.

NEW BUSINESS**6. Planning Commissioner / Subcommittee Reports****7. Report of the Community Development Director – 2025 Accomplishments****ADJOURNMENT**

Adjourn to the Planning Commission meeting of **November 11, 2025**, at 7:00 PM, in the City Hall Council Chambers, 70 North First Street, Campbell, California and via telecommunication

Americans with Disabilities Act (ADA)

In compliance with the Americans with Disabilities Act, listening assistance devices are available for meetings held in the Council Chambers. If you require accommodation to participate in the meeting, please contact the City Clerk's Office at ClerksOffice@campbellca.gov or 408-866-2117 in advance of the meeting.



**PLANNING COMMISSION
REGULAR MEETING MINUTES
Tuesday, September 23, 2025 | 7:00pm
City Hall Council Chamber**

CALL TO ORDER

The Regular Planning Commission meeting of September 23, 2025, was called to order at 7:00 pm by Chair Kamkar, and the following proceedings were followed

ROLL CALL

Planning Commissioners Present:

Matt Kamkar, Chair
Davis Fields, Vice Chair
Adam Buchbinder
Alan Zisser
Phil Alne
Gary Aquilina
Cori Majewski

Planning Commissioners Absent:

Staff Present:

Victoria Thompson, City Attorney's Office
Rob Eastwood, Director
Daniel Fama, Senior Planner
Stephen Rose, Senior Planner
Larissa Lomen, Assistant Planner

COMMUNICATIONS

None

AGENDA MODIFICATIONS OR POSTPONEMENTS

None.

ORAL COMMUNICATIONS

This portion of the meeting is reserved for individuals wishing to address the Planning Commission on matters of community concern that are not listed on the agenda. In the interest

of time, the Chair may limit speakers to five minutes. Please be aware that State law prohibits the Commission from acting on non-agendized items, however, the Chair may refer matters to staff for follow-up.

APPROVAL OF MINUTES

1. **Approval of Minutes of September 9, 2025** (*Roll Call Vote*)

Recommended Action: That the Planning Commission approve the regular meeting minutes of September 9, 2025.

Motion: Upon motion by Commissioner Zisser, seconded by Commissioner Majewski, the Planning Commission motioned to approve the regular meeting minutes of September 9, 2025, by the following roll call

AYES: Zisser, Majewski, Alne, Buchbinder, Aquilina, Fields, Kamkar

NOES: None

ABSTAIN: None

ABSENT: None

PUBLIC HEARING

2. **740 Craig Avenue – Variance** (Resolution/Roll Call Vote)

Public Hearing to consider the application of Adrian Javleo to allow retention of a driveway with a substandard side-yard setback constructed in association with a residential addition and remodel project, on property located at **740 Craig Avenue**. The application under consideration is a Variance. File No.: PLN-2025-101. Staff is recommending that this item be deemed Categorically Exempt from CEQA. Planning Commission action is final unless appealed in writing to the City Clerk within 10 calendar days. *Project Planner: Daniel Fama, Senior Planner*

Recommended Action: Adopt a Resolution approving a Variance.

Staff Presentation: Senior Planner Fama gave the staff presentation and answered questions from the Commission.

Applicant Testimony:

- The Applicants (Adrian Javleo and Lulu Tang) explained that a grade differential was discovered during construction, necessitating a retaining wall that reduced the usable side yard area.
- To support the Variance request, the Applicants provided Staff with a list of neighbors expressing their support.

Commission Discussion:

- Commissioners considered the merits and practicality of the 5-foot driveway separation requirement.
- Staff clarified that this Application should be evaluated solely on the relevant findings, and that the broader merits of the requirement will be reviewed at a future date.

Motion: Upon motion by Commissioner Aquilina, seconded by Commissioner Buchbinder, the Planning Commission motioned to approve a Variance to allow retention of a driveway with a substandard side-yard setback constructed in association with a residential addition and remodel project, on property located at 740 Craig Avenue File No.: PLN-2025-101, by the following roll call:

AYES: Zisser, Majewski, Alne, Buchbinder, Aquilina, Fields, Kamkar
NOES: None
ABSTAIN: None
ABSENT: None

3. **1300 Dell Avenue – Conditional Use Permit** (Resolution/Roll Call Vote)

Public Hearing to consider the application of Felicia Druar to allow for the continued operation of an existing large fitness studio (Sedusa Studios) on property located at **1300 Dell Avenue**. The application under consideration is a Conditional Use Permit. File No.: PLN-2025-10. Staff is recommending that this item be deemed Categorically Exempt from CEQA. Planning Commission action is final unless appealed in writing to the City Clerk within 10 calendar days. *Project Planner: Stephen Rose, Senior Planner*

Recommended Action: Adopt a Resolution approving a Conditional Use Permit (PLN-2025-10) to allow for the continued operation of an existing large fitness studio (Sedusa Studios).

Staff Presentation: Senior Planner Rose gave the staff presentation and answered questions from the Commission.

Applicant Testimony:

- The Applicant (Felicia Druar) stated that she purchased the business in 2017 and was not aware of the expiration terms of the prior CUP due to limited due diligence.
- The Applicant clarified that no food or alcohol is provided at any private events.

Commission Discussion:

- Commissioners discussed whether to approve the five-year term limit beginning from the expiration date of the previous CUP, as recommended in the staff report, or to follow “Alternative 1” in the staff report and begin the five-year term from the date of approval of the new CUP.
- Commissioners expressed concern that approving the latter option could set precedent and appear to reward applicants who allow their permits to lapse, and

suggested that the City improve its noticing practices to better inform applicants of expiration dates.

Motion: Upon motion by Commissioner Fields, seconded by Commissioner Zisser, the Planning Commission motioned to approve a Conditional Use Permit to allow the continued operation of an existing large studio (fitness training) (d.b.a. Sedusa Studios), for the maximum five (5) year term-limit permitted by code, on property located at 1300 Dell Avenue. File No.: PLN-2025-10, by the following roll call:

AYES: Zisser, Majewski, Alne, Buchbinder, Aquilina, Fields, Kamkar
NOES: None
ABSTAIN: None
ABSENT: None

NEW BUSINESS

4. **Planning Commissioner / Subcommittee Reports**
5. **Report of the Community Development Director**
 - Provided feedback on the ULI TAP for connecting the Downtown and the Pruneyard.

ADJOURNMENT

Adjourned at 8:30 PM to the Planning Commission meeting of **October 28, 2025**, at 7:00 PM, in the City Hall Council Chambers, 70 North First Street, Campbell, California and via telecommunication

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PREPARED BY: _____
Ken Ramirez, Administrative Analyst

APPROVED: _____
Matt Kamkar, Chair

ATTEST: _____
Rob Eastwood, Secretary



CITY OF CAMPBELL • PLANNING COMMISSION
Staff Report • October 28, 2025

PLN-2025-112

Scout Services

Public Hearing to consider the application of Scout Services to allow alterations to the Courtyard Marriott 162-room hotel, including reconstruction of an existing porte-cochere from a hipped roof to a flat-roof design, minor surface improvements, and a new building color scheme, on property located at **655 Creekside Way**, and superseding the property’s prior Planned Development Permit (PLN2006-179). The application under consideration is a Site and Architectural Review Permit. File No.: PLN-2025-112.

STAFF RECOMMENDATION

That the Planning Commission take the following actions:

1. **Adopt a Resolution** (reference **Attachment A**), approving a Site and Architectural Review Permit and superseding the property’s prior Planned Development Permit (PLN2006-179).

ENVIRONMENTAL (CEQA) DETERMINATION

Staff recommends that the Planning Commission accept the determination that this application is Categorically Exempt under Section 15301 (Class 1) of the California Environmental Quality Act (CEQA) pertaining to the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of an existing private structure.

PROJECT DATA

Zoning Designation:	P-D (Planned Development)
General Plan Designation:	General Commercial
Lot Area:	1.2 acres
Building Area:	95,400 square-feet
Building Height:	74-feet (measured to roof surface), 7-stories

DISCUSSION

Project Site: The project site is the Courtyard by Marriott hotel located along the west side of Creekside Way at the northbound Highway 17 off-ramp. The property abuts the 8x8 office building and parking garage to the south and is adjacent to the VTA Hamilton Avenue Light Rail station to the north, as shown on the map below. A hotel on this property was originally approved under a 1985 Planned Development Permit rezoning, which had also included the office buildings across Creekside Avenue, and the creation of Creekside Way itself. Various iterations of a hotel project on the property were proposed until the 2007 approval of a Planned Development Permit (PLN2006-179) finally resulted in the construction of the Marriott hotel as it exists today.



Proposed Project: The submitted application for a Site and Architectural Review Permit would allow alterations to the hotel, including reconstruction of the existing porte-cochere from a hipped roof to a flat-roof design, application of a new building color scheme, and minor surface improvements. The proposal is illustrated in the project plans (reference **Attachment B**) and in the architect’s design review deck (reference **Attachment C**), excerpted below, which highlights the evolution of the Marriott’s design style.



Since the porte-cochere, together with the complementary gazebo-like architectural feature above the hotel entry, serves as a defining visual element of the building’s overall composition, the proposed change constitutes a substantive alteration to the previously approved design. Given the prominence of this feature and its contribution to the building’s architectural identity, the modification warrants formal review by the Planning Commission, as further discussed under the “Findings for Approval” section.

Consideration in Review of Applications: The Planning Commission’s review of Site and Architectural Review Permits under CMC Section 21.42.040 requires consideration of various design, site layout, and operational aspects to ensure that proposed improvements contribute positively to the visual and functional character of the site. In this case, the project represents a focused architectural modification to an existing hotel, with no changes to site circulation, landscaping, or overall layout. The proposed alterations are consistent with Marriott’s current exterior design standards, which emphasize simplified forms, reduced ornamentation, and cooler, neutral color palettes.

The reconstruction of the porte-cochere and updated color palette reflect a deliberate effort to modernize the building’s appearance. Replacing the original hipped-roof porte-cochere with a flat-roof structure introduces a more contemporary, streamlined aesthetic while maintaining compatibility with the building’s overall composition. The new palette transitions from warm beige and red tones typical of mid-2000s design to layered grays that provide visual depth and a cohesive, understated tone consistent with surrounding development. Collectively, these improvements achieve a balanced modernization of the building’s exterior without altering its established form or function.

Findings for Approval: To grant a land use permit, the decision-making body must affirmatively establish that the project meets codified findings for approval. Findings establish the evidentiary basis for a City's decision to grant or deny a land use approval and to impose conditions of approval as necessary to establish the findings. The applicable findings depend upon the type of land use approval under review. The following analysis identifies each of the applicable findings in *italics* and how the proposed project satisfies them.

A. The project will be consistent with the general plan.

Yes. The General Plan land use designation for the site is *General Commercial*, intended to support commercial uses that benefit from high visibility to motorists and transit users, which includes hospitality uses. The proposed project is consistent with this designation and advances General Plan policies that encourage reinvestment in existing commercial centers. The proposed design improvements will refresh and modernize the hotel’s appearance, reinforcing its role as a high-quality commercial use within a prominent corridor.

- Policy LU-2.1** Promote high quality, creative design and site planning that is compatible with surrounding development, public spaces, and natural resources.
- Policy CD-1.3:** Through implementation of the city’s design review process, encourage creative, high-quality, innovative, and distinctive architectural and site designs that help create unique, vibrant places.
- Policy ED-2.4:** Encourage the revitalization of existing shopping centers, business parks, industrial areas, and key corridors as needed in order to meet the economic development goals of the city.

B. The project will be consistent with the Zoning Code;

Yes. The property is located within the former P-D (Planned Development) Zoning District, which has been designated a "legacy" district under the City’s updated Zoning Code (CMC Section 21.04.070). The hotel was originally approved under a Planned Development Permit (PLN2006-179), which has been reclassified as a legacy permit in accordance with CMC Section 21.56.060.E. Under the previous Zoning Code, the proposed architectural change would have been processed as a minor amendment to the Planned Development Permit.

However, because the current Zoning Code no longer provides a mechanism to modify legacy permits, these changes now require a new discretionary entitlement. Approval of this Site and Architectural Review Permit will therefore supersede the prior Planned Development authorization, bringing the property into regulatory alignment with the current Zoning Code while maintaining the existing hotel use, development intensity, and overall site configuration.

C. The project will aid in the harmonious development of the immediate area; and

Yes. The proposed architectural modifications will enhance the visual quality of the hotel while maintaining compatibility with the surrounding commercial and hospitality context. The reconstruction of the porte-cochere with a simplified flat-roof form and the application of a cooler, neutral color palette will create a more cohesive and contemporary appearance that aligns with nearby development and current Marriott brand standards. These improvements will refresh the building's street presence and contribute to the continued high-quality character and visual harmony of the corridor.

D. The project is consistent with applicable adopted design guidelines, development agreement, overlay district, area plan, neighborhood plan, and specific plan(s).

Not Applicable. Although the property is located within the Pruneyard/Creekside Commercial District, which is identified in the General Plan as a "special planning area," no specific design guidelines, area plan, or overlay district regulations have been adopted for this area. However, the project is consistent with the intent of Goal LU-8, which calls for high-quality design and ongoing enhancement of the Pruneyard/Creekside District as a vibrant mixed-use area. The proposed architectural improvements will refresh and modernize the hotel's appearance while maintaining compatibility with nearby commercial and hospitality development, thereby supporting the District's objective of promoting a cohesive and visually engaging environment.

Site and Architectural Review Committee: The Site and Architectural Review Committee (SARC) reviewed this application at its meeting of September 23, 2025. The Committee was generally supportive of the proposed architectural updates and recognized the intent to modernize the building consistent with the Marriott brand. The Committee discussed the proposed color palette, noting its subdued and neutral character, and suggested that additional contrast or accent tones could enhance visual interest, particularly on the northeast elevation. The Committee also acknowledged that the revised porte-cochere design represents a contemporary shift from the original style, but encouraged greater visual differentiation, particularly along the northeast elevation.

Signage: The new signage proposed as part of the remodel will either be subject to administrative review and approval under the City's Sign Ordinance or installed in accordance with the prior City Council approval governing freeway-oriented signage for the site.

Public Comment: No public comment was received.

Attachments:

- A. Draft Resolution
- B. Project Plans
- C. Architect's Design Review Deck

Prepared by: 

Daniel Fama, Senior Planner

Approved by: 

Rob Eastwood, Community Development Director

RESOLUTION NO. 2025-xx

BEING A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CAMPBELL GRANTING APPROVAL OF A SITE AND ARCHITECTURAL REVIEW PERMIT TO ALLOW ALTERATIONS TO THE COURTYARD MARRIOTT 162-ROOM HOTEL, INCLUDING RECONSTRUCTION OF AN EXISTING PORTE-COCHERE FROM A HIPPED ROOF TO A FLAT-ROOF DESIGN, MINOR SURFACE IMPROVEMENTS, AND A NEW BUILDING COLOR SCHEME, ON PROPERTY LOCATED AT **655 CREEKSIDE WAY**, AND SUPERSEDING THE PROPERTY'S PRIOR PLANNED DEVELOPMENT PERMIT (PLN2006-179). FILE NO.: PLN-2025-112

After notification and public hearing, as specified by law and after presentation by the Community Development Director, proponents and opponents, the hearing was closed.

The Planning Commission finds as follows with regard to the approval of a Site and Architectural Review Permit:

1. The project site is the Courtyard by Marriott Hotel, located along the west side of Creekside Way at the northbound Highway 17 off-ramp.
2. The project site is located within the Planned Development (P-D) Zoning District, as shown on the City of Campbell Zoning Map.
3. The project site is designated General Commercial (GC) on the City of Campbell General Plan Land Use Diagram.
4. The property is developed with an existing seven-story hotel, originally approved under Planned Development Permit PLN2006-179, authorized by City Council Ordinance No. 2086 (2007).
5. The proposed project is an application for a Site and Architectural Review Permit to allow reconstruction of the existing porte-cochere with a simplified flat-roof design, application of a new exterior color palette, and related architectural refinements consistent with current Marriott brand standards.
6. The Planned Development (P-D) District has been designated a legacy district pursuant to CMC Section 21.04.070, and all previously approved Planned Development Permits have been reclassified as legacy permits under CMC Section 21.56.060.E. Legacy permits may not be amended and must be superseded by a new discretionary entitlement to authorize further modification under the current Zoning Code.
7. While not a formal revocation, this Site and Architectural Review Permit supersedes the prior Planned Development Permit (PLN2006-179) by incorporating the originally approved plans into the current approval and rendering void its original conditions, thereby achieving the same functional outcome of enabling future regulation under the current Zoning Code.

8. The project will not alter established site circulation, landscaping, or building layout, and the architectural improvements will enhance the building's appearance while maintaining compatibility with the surrounding commercial context.
9. The proposed project is consistent with the intent of Goal LU-8 (Pruneyard/Creekside Commercial District) and applicable General Plan policies LU-2.1, CD-1.3, and ED-2.4, which encourage high-quality architectural design and reinvestment in existing commercial centers.

Policy LU-2.1: Promote high quality, creative design and site planning that is compatible with surrounding development, public spaces, and natural resources.

Policy CD-1.3: Through implementation of the city's design review process, encourage creative, high-quality, innovative, and distinctive architectural and site designs that help create unique, vibrant places.

Policy ED-2.4: Encourage the revitalization of existing shopping centers, business parks, industrial areas, and key corridors as needed in order to meet the economic development goals of the city.

10. In review of the Proposed Project, the Planning Commission considered traffic safety, traffic congestion, site circulation, adequacy of landscaping, and the appropriateness of proposed structures and site layout, pursuant to Campbell Municipal Code Sec. 21.42.040 (Considerations in review of applications).
11. There is a reasonable relationship and a rough proportionality between the Conditions of Approval and the impacts of the project.
12. No substantial evidence has been presented which shows that the project, as currently presented and subject to the required conditions of approval, will have a significant adverse impact on the environment.
13. There is a reasonable relationship between the use of the fees imposed upon the project and the type of development project.

Based upon the foregoing findings of fact, the Planning Commission further finds and concludes that:

Site and Architectural Review Permit Findings (CMC Sec. 21.42.060.B):

1. The project will be consistent with the general plan;
2. The project conforms with the Zoning Code;
3. The project will aid in the harmonious development of the immediate area;
4. The project is consistent with applicable adopted design guidelines, development agreement, overlay district, area plan, neighborhood plan, and specific plan(s);\

Environmental Findings (CMC Sec. 21.38.050):

5. The project is Categorically Exempt under Section 15301 (Class 1) of the California Environmental Quality Act (CEQA) pertaining to the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of an existing private structure; and
6. No substantial evidence has been presented which shows that the project, as currently presented and subject to the required conditions of approval, will have a significant adverse impact on the environment.

THEREFORE, BE IT RESOLVED that the Planning Commission approves a Site and Architectural Review Permit to allow alterations to the Courtyard Marriott 162-room hotel, including reconstruction of an existing porte-cochere from a hipped roof to a flat-roof design, minor surface improvements, and a new building color scheme, on property located at **655 Creekside Way**, and superseding the property's prior Planned Development Permit (PLN2006-179), subject to the attached Conditions of Approval (attached **Exhibit A**).

PASSED AND ADOPTED this 8th day of October 28, 2025, by the following roll call vote:

AYES: Commissioners:
NOES: Commissioners:
ABSENT: Commissioners:
ABSTAIN: Commissioners:

APPROVED: _____
Matt Kamkar, Chair

ATTEST: _____
Rob Eastwood, Secretary

CONDITIONS OF APPROVAL
PLN-2025-112

Where approval by the Director of Community Development, City Engineer, Public Works Director, City Attorney or Fire Department is required, that review shall be for compliance with all applicable conditions of approval, adopted policies and guidelines, ordinances, laws and regulations and accepted engineering practices for the item under review. Additionally, the applicant is hereby notified that he/she is required to comply with all applicable Codes or Ordinances of the City of Campbell and the State of California that pertain to this development and are not herein specified.

PLANNING DIVISION

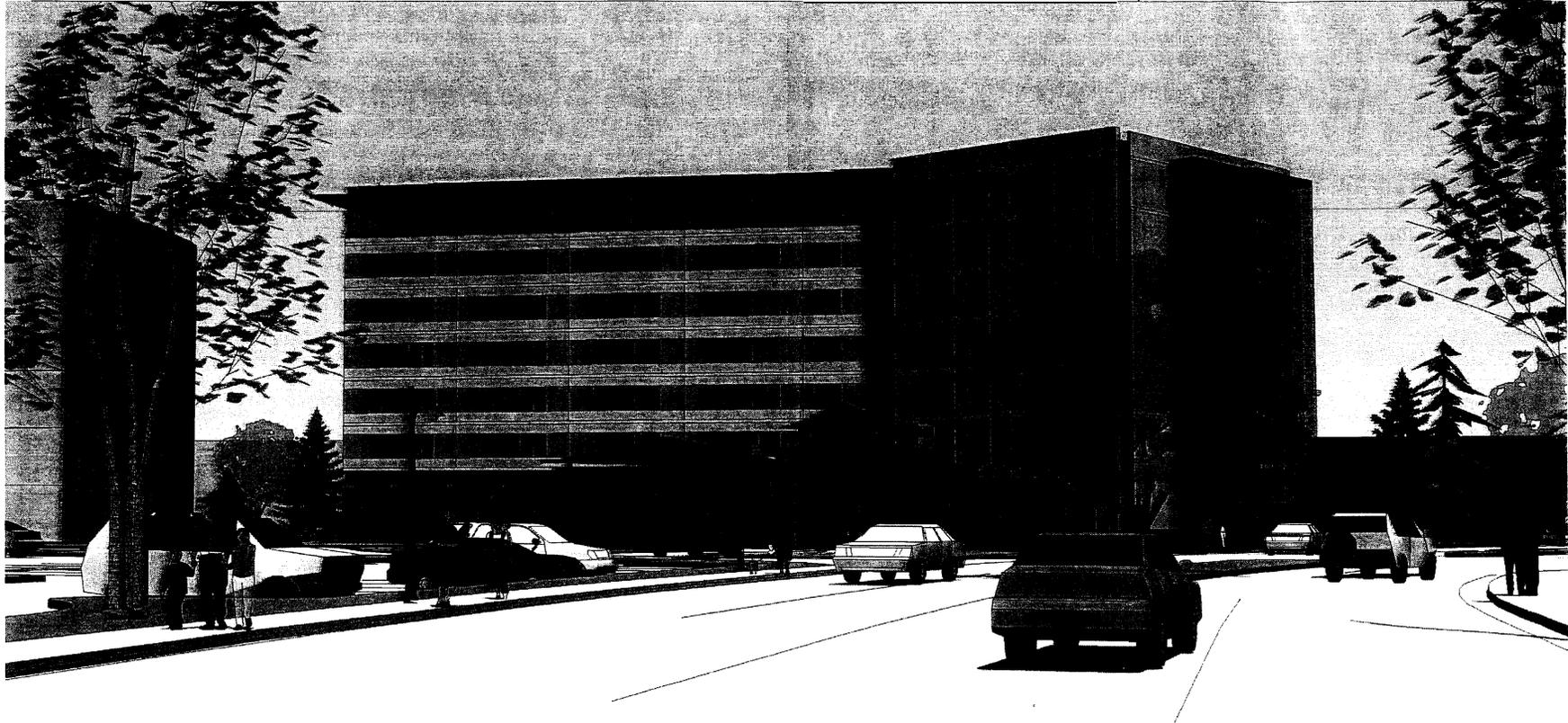
1. Approved Project: Approval is granted for a Site and Architectural Review Permit to allow alterations to the Courtyard Marriott 162-room hotel, including reconstruction of an existing porte-cochere from a hipped roof to a flat-roof design, minor surface improvements, and a new building color scheme, on property located at **655 Creekside Way**, as depicted in the project plans included as Attachment B to the October 28, 2025, Planning Commission staff report, and, by this action, superseding the property's prior Planned Development Permit (PLN2006-179). The hotel building shall otherwise remain in substantial conformance with the project plans prepared by Steinberg Architects, dated as received by the Planning Division on April 11, 2006, and approved by City Council Ordinance No. 2086, which are incorporated herein by reference as Exhibit B.
2. Approval Establishment / Expiration: The Site and Architectural Review Permit approved herein ("Approval") shall be valid for one (1) year from the effective date of Planning Commission approval, expiring on November 7, 2026. To establish the Approval, a building permit for construction of the approved building alterations and site improvements shall be issued within that one-year period, pursuant to Campbell Municipal Code (CMC) Section 21.56.030.B.1 (Issuance of Building Permit). Failure to obtain a building permit within this timeframe, or expiration of an issued building permit, shall result in the Approval being deemed void.
3. Previous Conditions of Approval: Upon establishment of this Approval, the Conditions of Approval previously adopted by City Council Ordinance No. 2086 shall be deemed void and permanently superseded in their entirety by the Conditions of Approval set forth herein, except in the event this Approval expires or is rendered void pursuant to Condition of Approval No. 2 (Approval Establishment / Expiration).
4. Signage: No signage is approved as part of this Approval. Any new signage shall be subject to a new Sign Permit approved in compliance with CMC Section 21.30.080 (Permanent Signs), or shall be installed in conformance with the previously approved Sign Permit (PLN2009-51), granted by City Council Resolution No. 11062.
5. Conditional Use Permit Approval: This Approval shall have no effect on Conditional Use Permit PLN2009-02, approved by Planning Commission Resolution No. 3928, which authorizes the provision of alcoholic beverage service within the hotel. That permit remains in full force and effect under the conditions of approval set forth therein.
6. Lodging Duration: Guest stays shall be limited to a maximum of thirty (30) consecutive days, consistent with the definition of "hotel" pursuant to CMC Section 21.72.020.

7. Prior Agreements: This Approval shall not affect, nullify, amend, or otherwise modify any existing parking rights or obligations established by recorded agreements, easements, or covenants applicable to any abutting or adjacent properties. All such instruments shall remain in full force and effect in accordance with their terms.
8. Minor Revisions: Architectural refinements and other minor revisions to the Approved Project Plans may be administratively reviewed by the Community Development Director pursuant to CMC Section 21.56.060 (Amendments to an approved project).
9. Lighting Design: Any new exterior lighting fixtures shall be designed in compliance with the City of Campbell Lighting Design Standards (CMC Section 21.18.090).
10. Refuse Management: Refuse and recycling receptacles shall be kept within the solid waste enclosure at all times, except during collection, in compliance with CMC Chapter 6.04 (Garbage and Rubbish Disposal). The enclosure doors shall remain closed at all times, except when receptacles are being moved.
11. Property Maintenance: All exterior areas of the property shall be kept free of graffiti, trash, rubbish, and litter in compliance with CMC Section 21.16.110 (Site Maintenance).
12. Landscape Maintenance: All landscaping shall be maintained in compliance with Campbell Municipal Code Section 21.26.040 (Landscaping Maintenance Requirements) and Section 21.16.110.C.2 (Landscape Maintenance). Dead or unhealthy plants shall be replaced with healthy plants of the same or similar type.
13. Outdoor Storage: No outdoor storage is permitted on the subject property. All equipment, materials, and business-related items shall be stored within approved enclosed structures.
14. Parking and Driveways: All parking and driveway areas shall be maintained in compliance with the standards provided in CMC Ch. 21.28 (Parking and Loading). Parking spaces shall be free of debris or other obstructions.
15. Smoking: "No Smoking" signs shall be posted on the premises in compliance with CMC Section 6.11.060. Smoking shall be prohibited on the property and on the public sidewalk within twenty feet of all buildings. It is the responsibility of the establishment owner to ensure patron compliance with this restriction.
16. Fences/Walls: All newly proposed fencing and/or walls shall comply with CMC Section 21.18.060. The existing sound wall shall be maintained in compliance with the design approved by Building Permit No. BLD2009-00424.

COUNTY FIRE DISTRICT

17. Fire Sprinklers Modification: Provide a submittal to SCCFD for the installation/modification of each of the systems listed below per CFC Chapter 1 and 9. Submit shop drawings (3 sets) and a permit application to the Santa Clara County Fire Department for approval before altering the systems as applicable. Call (408) 341-4420 for more information. NOTE: Proof that the corresponding building permit has been formally issued shall be provided at time of submittal.

HILTON GARDEN INN - CAMPBELL, CALIFORNIA



VICINITY MAP	PROJECT DATA	PROJECT DESCRIPTION	SHEET INDEX																								
	<p>ADDRESS : 649 CREEKSIDE WAY, CAMPBELL</p> <p>TOTAL NUMBER OF HOTEL GUEST ROOMS: 162</p> <p>ACCESSIBILITY DATA:</p> <ul style="list-style-type: none"> REGULAR UNIT NET AREAS(143): 327 S.F. EACH ACCESSIBLE UNITS NET AREAS (7): 446 S.F. EACH FULLY ACCESSIBLE UNITS : 7 (LEVEL 2-7) FULLY ACCESSIBLE UNITS WITH ROLL-IN SHOWERS : 2 (LEVEL 2) <p>PARKING:</p> <table border="1"> <tr> <td>LOT 1:</td> <td>83</td> </tr> <tr> <td>LOT 2:</td> <td>86</td> </tr> <tr> <td>HANDICAPPED STALLS:</td> <td>5</td> </tr> <tr> <td>H.C. VAN ACCESSIBLE STALLS:</td> <td>1</td> </tr> <tr> <td>TOTAL PARKING :</td> <td>177</td> </tr> </table> <p>AREA SUMMARY:</p> <table border="1"> <tr> <td>TOTAL PARCEL AREA:</td> <td>52,664 S.F.</td> </tr> <tr> <td>TOTAL GUEST ROOMS NET AREA:</td> <td>53,308 S.F.</td> </tr> <tr> <td>TOTAL HEIGHT:</td> <td>75 FEET</td> </tr> <tr> <td>TOTAL GROSS AREA GROUND FLOOR:</td> <td>15,946 S.F.</td> </tr> <tr> <td>TOTAL GROSS AREA PER TYPICAL LEVEL IN TOWER:</td> <td>13,253 S.F. (x6)</td> </tr> <tr> <td>TOTAL BUILDING AREA:</td> <td>95,183 S.F.</td> </tr> <tr> <td>FLOOR AREA RATIO:</td> <td>1.80</td> </tr> </table>	LOT 1:	83	LOT 2:	86	HANDICAPPED STALLS:	5	H.C. VAN ACCESSIBLE STALLS:	1	TOTAL PARKING :	177	TOTAL PARCEL AREA:	52,664 S.F.	TOTAL GUEST ROOMS NET AREA:	53,308 S.F.	TOTAL HEIGHT:	75 FEET	TOTAL GROSS AREA GROUND FLOOR:	15,946 S.F.	TOTAL GROSS AREA PER TYPICAL LEVEL IN TOWER:	13,253 S.F. (x6)	TOTAL BUILDING AREA:	95,183 S.F.	FLOOR AREA RATIO:	1.80	<p>THE PROPOSED DEVELOPMENT IS A 7 STORY BUILDING TO HOUSE A HILTON GARDEN INN HOTEL. THE BUILDING TOWER WILL HAVE 162 GUEST ROOMS. THE GROUND FLOOR WILL HAVE CONFERENCE ROOMS, RESTAURANT, OFFICES, POOL AND OTHER GATHERING SPACES.</p> <p>THE BUILDING WILL BE A TYPE I NON-COMBUSTIBLE STRUCTURE, 75 FEET TALL.</p> <p>APPLICANT: CAMPBELL CREEKSIDE, LLC SAND HILL PROPERTY COMPANY 489 SOUTH EL CAMINO REAL SAN MATEO, CA 94402</p> <p>ARCHITECT: STEINBERG ARCHITECTS 60 PIERCE AVENUE SAN JOSE, CALIFORNIA 95110</p>	<p>A0.1 - COVER SHEET</p> <p>A0.2 - SITE PHOTOS</p> <p>A1.1 - SITE PLAN</p> <p>A1.2 - SITE PLAN - GROUND LEVEL</p> <p>A2.1 - BUILDING PLAN - FIRST LEVEL</p> <p>A2.2 - BUILDING PLAN - SECOND LEVEL</p> <p>A2.3 - BUILDING PLAN - TYP. 3RD - 7TH LEVEL</p> <p>A3.1 - BUILDING SECTION - SECTION A-A</p> <p>A3.2 - BUILDING ELEVATIONS / SOUTH ELEVATION</p> <p>A3.3 - BUILDING ELEVATIONS / EAST ELEVATION</p> <p>A3.4 - BUILDING ELEVATIONS / NORTH ELEVATION</p> <p>A3.5 - BUILDING ELEVATIONS / WEST ELEVATION</p> <p>A3.6 - WALL SECTIONS</p> <p>A4.1 - GUEST ROOMS</p> <p>C1 - GRADING AND DRAINAGE</p> <p>L1 - CONCEPTUAL LANDSCAPE PLAN</p> <p>L2 - HOTEL LANDSCAPE PLAN</p>
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 CITY OF CAMPBELL
 PLANNING DEPT.



PROJECT: Steinberg Architects
 60 Pierce Avenue
 San Jose CA 95110

HILTON GARDEN INN
 CAMPBELL, CALIFORNIA



STEINBERG ARCHITECTS

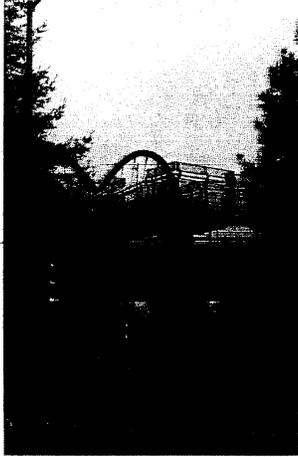
PROJECT NO: 02025 DATE: 04.10.2007

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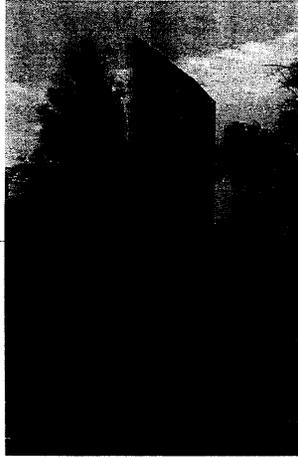
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COVER SHEET
A0.1

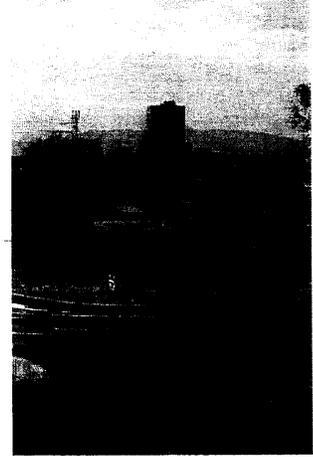
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1 TRAIN STATION
NEIGHBOR PROPERTY (NORTH)



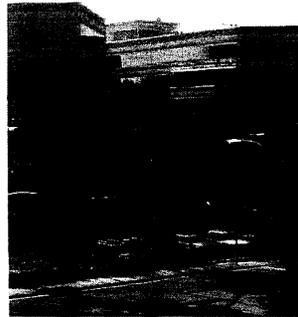
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NEIGHBOR PROPERTY (NORTH)



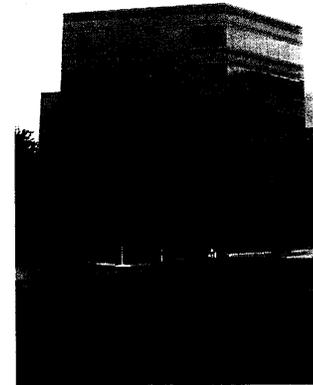
3 SITE
LOOKING SOUTH



4 SITE AND HIGHWAY EXIT
LOOKING SOUTH



5 OFFICE BUILDING
NEIGHBOR PROPERTY (EAST)



6 OFFICE BUILDING
NEIGHBOR PROPERTY (EAST)

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architect Steinberg Architects
60 Pierce Avenue
San Jose CA 95110

HILTON GARDEN INN
CAMPBELL, CALIFORNIA

ONE-HILL PROPERTY COMPANY
50 East 4th Avenue
SAN MATEO, CA 94401



STEINBERGARCHITECTS

project no: 05026 date: 04.10.2007
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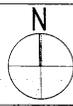
SITE PHOTO
A0.2

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CONSULT THE CITY OF SAN MATEO PLANNING DEPARTMENT FOR THE CITY'S COMMENTS ON THE PLANS. THE CITY'S COMMENTS ARE FOR INFORMATION ONLY AND DO NOT CONSTITUTE AN ENDORSEMENT OF THE ARCHITECT'S WORK.

ALL DIMENSIONS ARE IN FEET AND INCHES. DIMENSIONS ARE SHOWN TO THE CENTERLINE UNLESS OTHERWISE NOTED.



ARCHITECT
 Steinberg Architects
 60 Pierce Avenue
 San Jose CA 95110

HILTON GARDEN INN
 CAMPBELL, CALIFORNIA

PROJECT NUMBER: 05026
 30 East 4th Avenue
 SAN MATEO, CA 94401



STEINBERG ARCHITECTS

PROJECT NO: 05026 DATE: 04.10.2007
 DRAWN BY: LK CHECKED BY: JH

SCALE: 1/8" = 1'-0"

TYPICAL PLAN - 370 TO 370/02

A2.3



SOUTH ELEVATION
1/8" = 1' - 0"

ARCHITECT Steinberg Architects
603 Pierce Avenue
San Jose CA 95110

HILTON GARDEN INN
CAMPBELL, CALIFORNIA

SHED-ALL PROPERTY COMPANY
30 East 4th Avenue
SAN MATEO, CA 94401

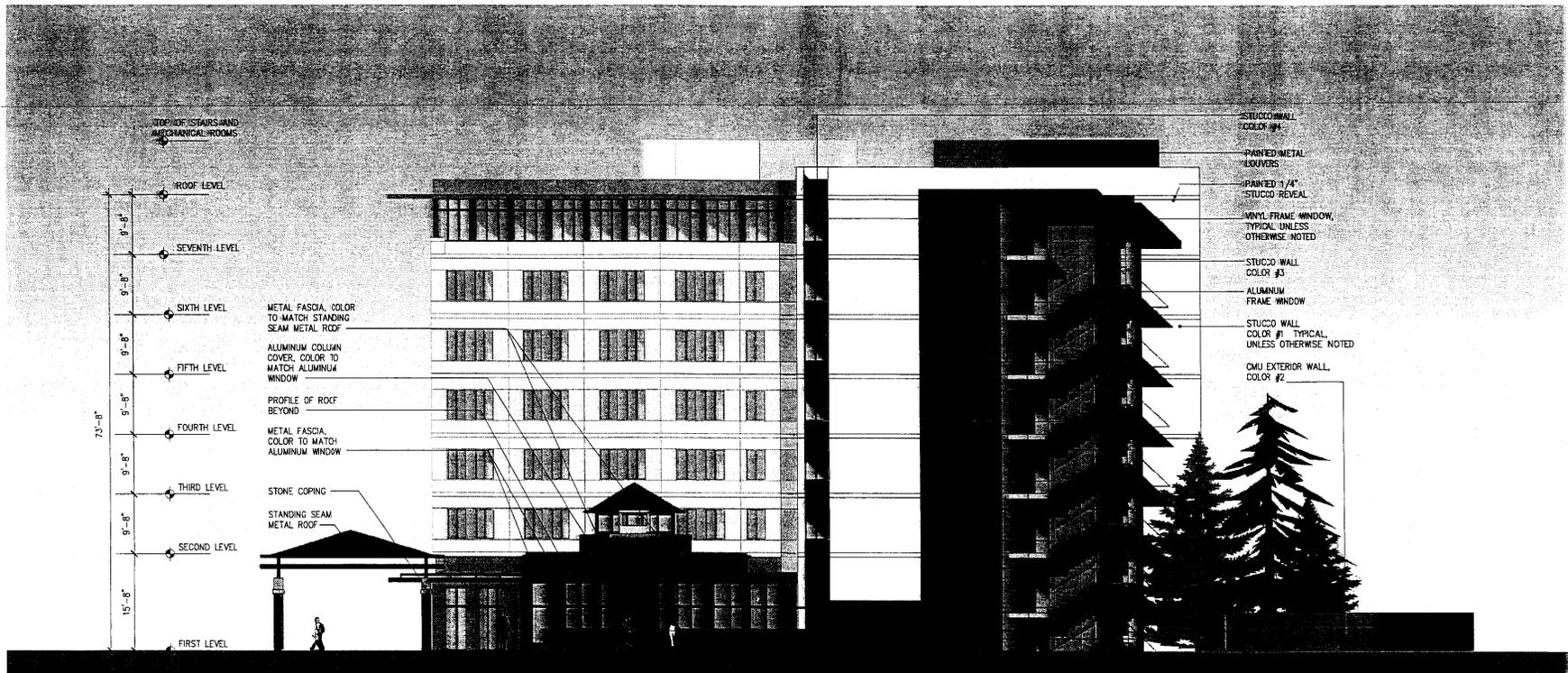


STEINBERG ARCHITECTS

PROJECT: 05203 DATE: 04.10.2007
DRAWN BY: JJ CHECKED BY: JG
SCALE: 1/8" = 1' - 0"

SOUTH ELEVATION

A3.2



EAST ELEVATION
 1/16" = 1' - 0"

architect Steinberg Architects
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 San Jose CA 95110

HILTON GARDEN INN
 CAMPBELL, CALIFORNIA

SHOULL PROPERTY COMPANY
 30 East 4th Avenue
 SAN MATEO, CA 94401

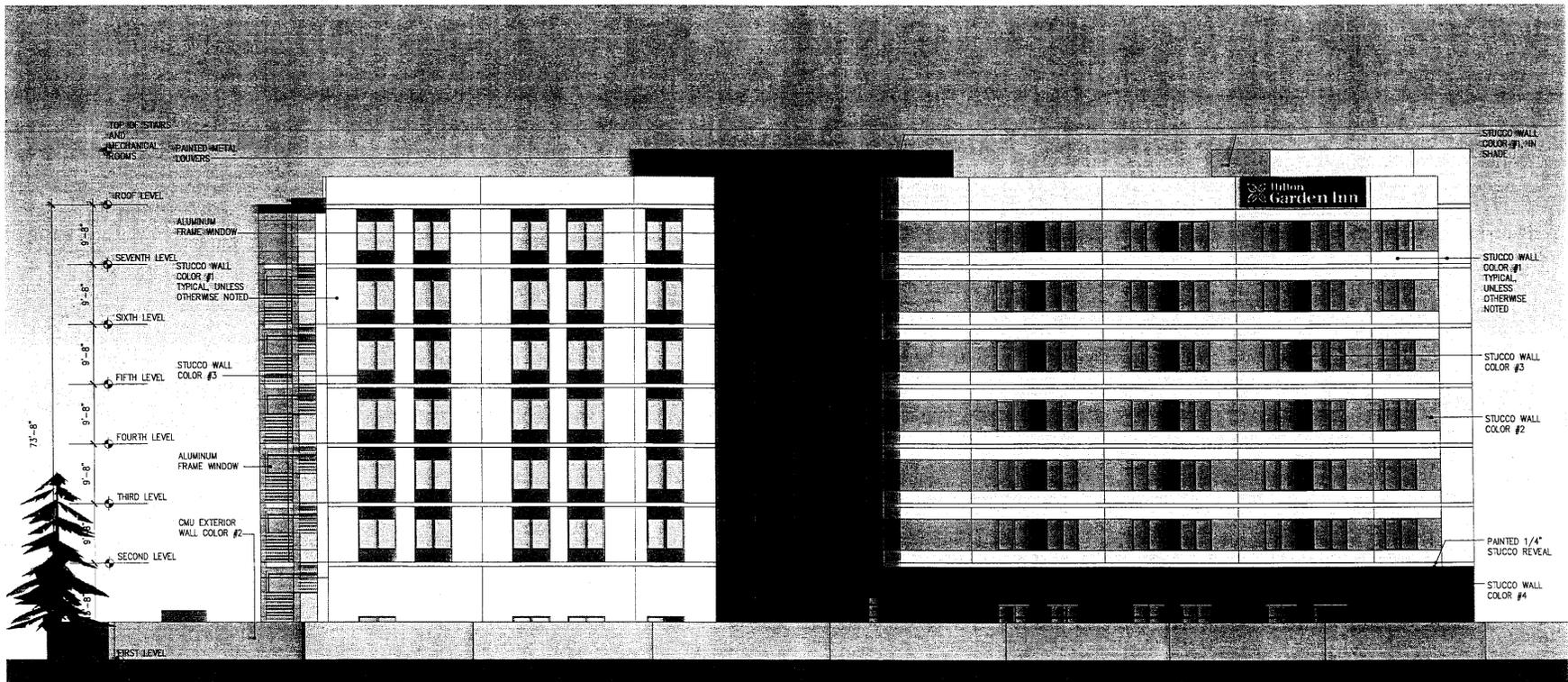


STEINBERGARCHITECTS

drawn by 0502E date 04.10.2007
 checked by JL
 scale 1/16" = 1' - 0"

BUILDING ELEVATION
 DISTRIBUTION

A3.3



NORTH ELEVATION
1/16" = 1' - 0"

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San Jose CA 95110

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CAMPBELL, CALIFORNIA

THE HILL PROPERTY COMPANY
30 East 9th Avenue
SAN MATEO, CA 94401



STEINBERGARCHITECTS

project no: 05020 date: 04-10-2007
drawn by: LJ checked by: JC
scale: 1/16" = 1'-0"

SCALE ELEVATION
NOT TO SCALE
A3.4



WEST ELEVATION
1/16" = 1' - 0"

architect Steinberg Architects
60 Pierce Avenue
San Jose, CA 95110

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CAMPELL, CALIFORNIA

S&W REAL PROPERTY COMPANY
20 East 4th Avenue
SAN MATEO, CA 94401



STEINBERGARCHITECTS

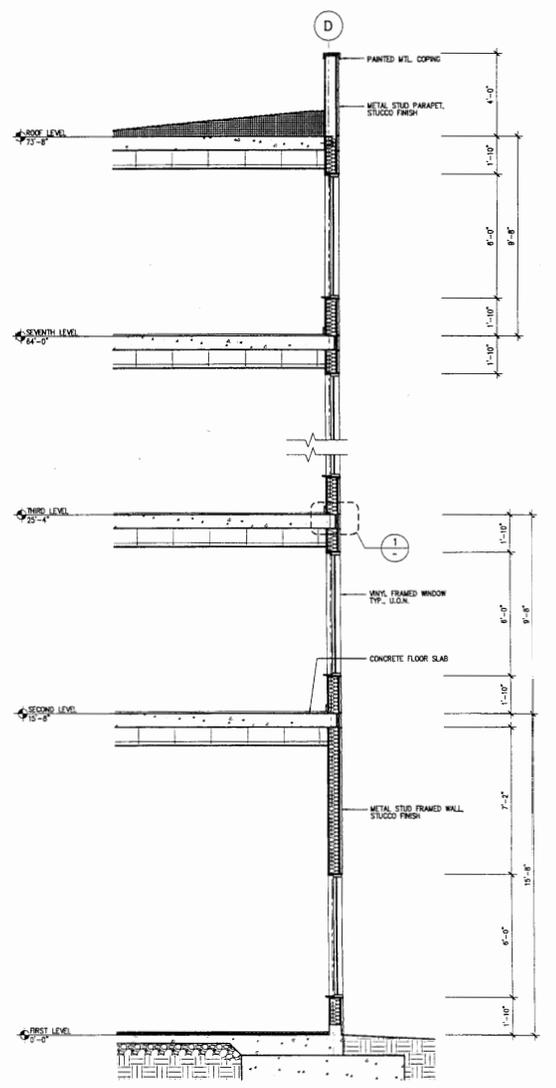
project no: 05026 date: 04.10.2007
drawn by: JG checked by: JG
scale: 1/16" = 1' - 0"

HILTON GARDEN INN
WEST ELEVATION

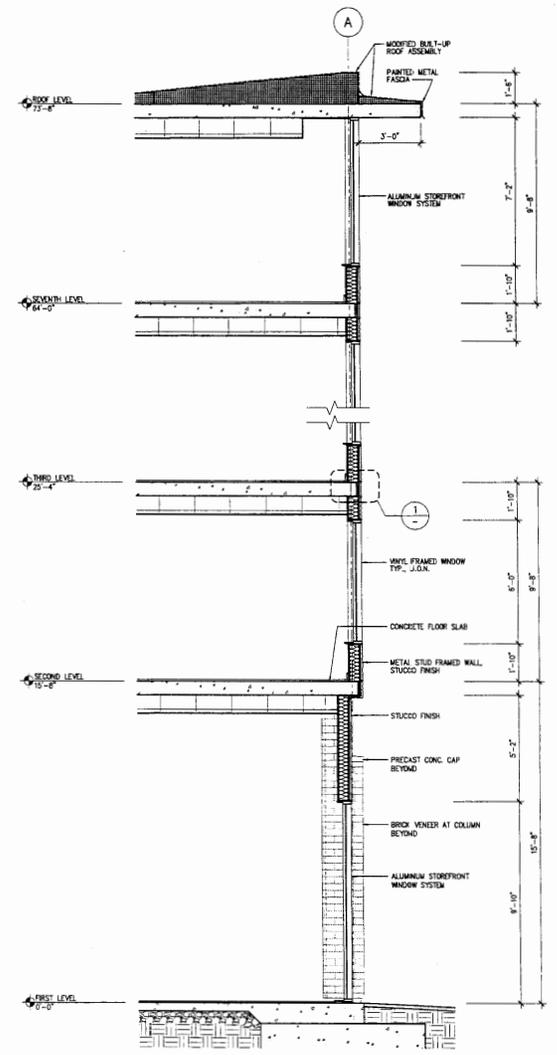
A3.5

CONSULT THE ARCHITECT FOR THE LOCATION OF THE WALL SECTION. THE WALL SECTION IS TO BE TAKEN AT THE LOCATION SHOWN ON THE ARCHITECTURAL DRAWINGS.

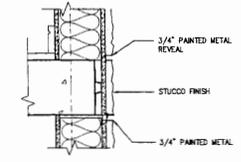
ALL DIMENSIONS ARE IN FEET AND INCHES. DIMENSIONS IN PARENTHESES ARE IN FEET. DIMENSIONS IN BRACKETS ARE IN INCHES.



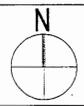
TYP. WALL SECTION AT N. ELEV. 3/8" = 1'-0" ③



TYP. WALL SECTION AT S. ELEV. 3/8" = 1'-0" ②



TYP. EDGE OF SLAB 1-1/2" = 1'-0" ①



ARCHITECT
 Steinberg Architects
 80 Pierce Avenue
 San Jose CA 95110

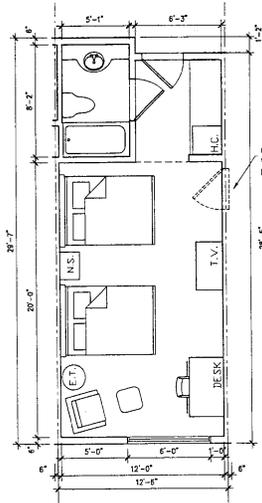
HILTON GARDEN INN
 CAMPBELL, CALIFORNIA



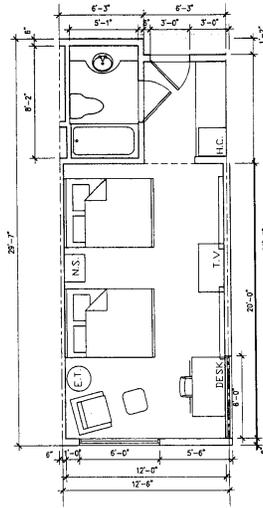
PROJECT NO. 00278 DATE 04.10.2007
 DRAWN BY: JJ CHECKED BY: JC
 DATE: AS DEFINED

WALL SECTIONS
A3.6

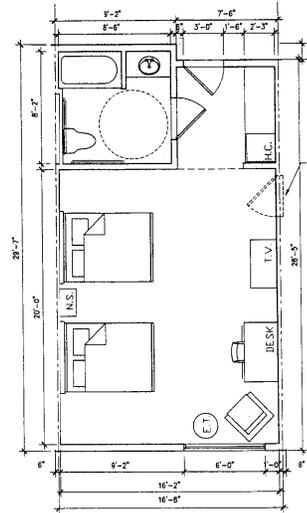
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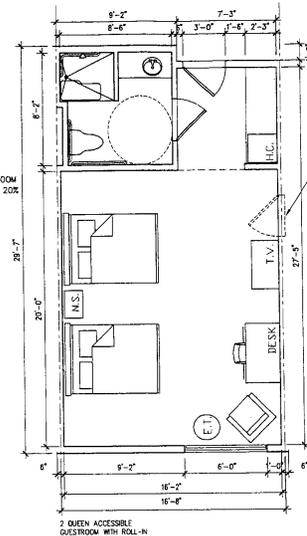
1 TYPICAL GUEST ROOM
 143 UNITS 332 S.F. EACH



2 GUEST ROOM WITH CORNER WINDOW
 5 UNITS 444 S.F. EACH



3 ACCESSIBLE GUEST ROOM
 5 UNITS 444 S.F. EACH



4 ACCESSIBLE GUEST ROOM WITH ROLL-IN-SHOWER
 2 UNITS 444 S.F. EACH

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architect: Steinberg Architects
 60 Pierce Avenue
 San Jose CA 95110

HILTON GARDEN INN
 CAMPBELL, CALIFORNIA

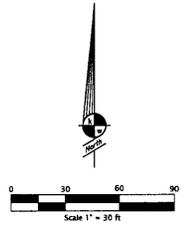
SHW REAL PROPERTY COMPANY
 30 East 4th Avenue
 SAN MATEO, CA 94401



STEINBERGARCHITECTS

project no: 05025 date: 04.10.2007
 drawn by: MJ checked by: JF
 scale: 1/4"=1'-0"

UNIT PLANS
A4.1



HIGHWAY 17 NORTHBOUND

HIGHWAY 17 OFF-RAMP

HOTEL
FF 181.0

CREKESIDE WAY

ABBREVIATIONS

- ABS ACETYL BUTYL STYRENE
- AC ASPHALTIC CONCRETE
- BTM BOTTOM
- BW BACK OF WALK
- CB CATCH BASIN
- CONST. CONSTRUCT
- DI DROP INLET
- EB ELECTRIC BOX
- EL ELECTRODER
- EX. or FD EXISTING
- FF FINISH FLOOR
- FL FLOW LINE
- GB GRADE BREAK
- HP HIGH POINT
- INV INVERT ELEVATION
- IRK IRRIGATION
- JB JUNCTION BOX
- LIP LIP OF GUTTER
- PV PAVEMENT
- RCP REINFORCED CONCRETE PIPE
- RDWD REDWOOD
- RM RIM ELEVATION
- ROW RIGHT OF WAY
- SD STORM DRAIN
- SDMH STORM DRAIN MANHOLE
- SLB STREET LIGHT BOX
- SS SANITARY SEWER
- SSMH SANITARY SEWER MANHOLE
- STM STORM
- TC TOP OF CURB
- TOP TOP OF SLOPE
- TSB TRAFFIC SIGNAL BOX
- VCP VITRIFIED CLAY PIPE
- WM WATER METER

LEGEND

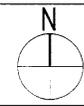
	PROPOSED	EXISTING
PROPERTY LINE	---	---
CENTERLINE	---	---
EASEMENT	---	---
NON-ACCESS	---	---
CLANHOOT	---	---
ELECTRODER	---	---
FIR: HYDRANT	---	---
FND: STANDARD CITY MONUMENT	---	---
POST	---	---
SANITARY SEWER MANHOLE	---	---
SIGN	---	---
SPT ELEVATION	---	---
STORM DRAIN MANHOLE	---	---
TRIE	---	---
TRIE TO BE REMOVED	---	---
UTILITY POLE	---	---
VALVE	---	---
CATCH BASIN (CB)	---	---
JUNCTION BOX (JB)	---	---
VERTICAL CURB	---	---
CURB & GUTTER	---	---
FENCE	---	---
TRISS AND BRUSH	---	---
UNDERGROUND ELECTRIC	---	---
GAS	---	---
JOINT TRENCH	---	---
OVERHEAD	---	---
SANITARY SEWER	---	---
STORM DRAIN	---	---
TELEPHONE	---	---
WATER	---	---
STREET LIGHT CONDUIT	---	---
STORM DRAIN OVERLAND RELEASE	---	---
RIDGE	---	---
2.5" AC ON 11.5" CL 3" AB	---	---
3.0" AC ON 10" CL 3" AB	---	---
CONCRETE	---	---

M.F.P. C. P.T.L. C. D.V.C. C. B.M.P. A.S.M.D. A.S.M.D. S.T.R.A. B.S.A.L.P.R. C.E.T. M.A.N.A.G.E.R. I.P.R.O.J.E.C.T.S. 151.47.12 (M)G/O.V.A.L.U.E.T.E.C. C.A.O.D. D.W. N.O.VEMBER 07. 2006 10:51 AM

S.H.A.R.P. C.A.M.P. 151.47.12 (M)G/O.V.A.L.U.E.T.E.C. C.A.O.D. D.W. N.O.VEMBER 07. 2006 10:51 AM

KIER & WRIGHT
 CIVIL ENGINEERS & SURVEYORS, INC.
 3330 Scott Boulevard, Building 22
 Santa Clara, California 95054
 (408) 727-6862
 fax (408) 727-5841

DATE: MAR. 28. 2006
 SCALE: AS SHOWN
 DESIGNER: M. KNUDSEN
 DRAFTER: M. KNUDSEN
 JOB: 95147-12



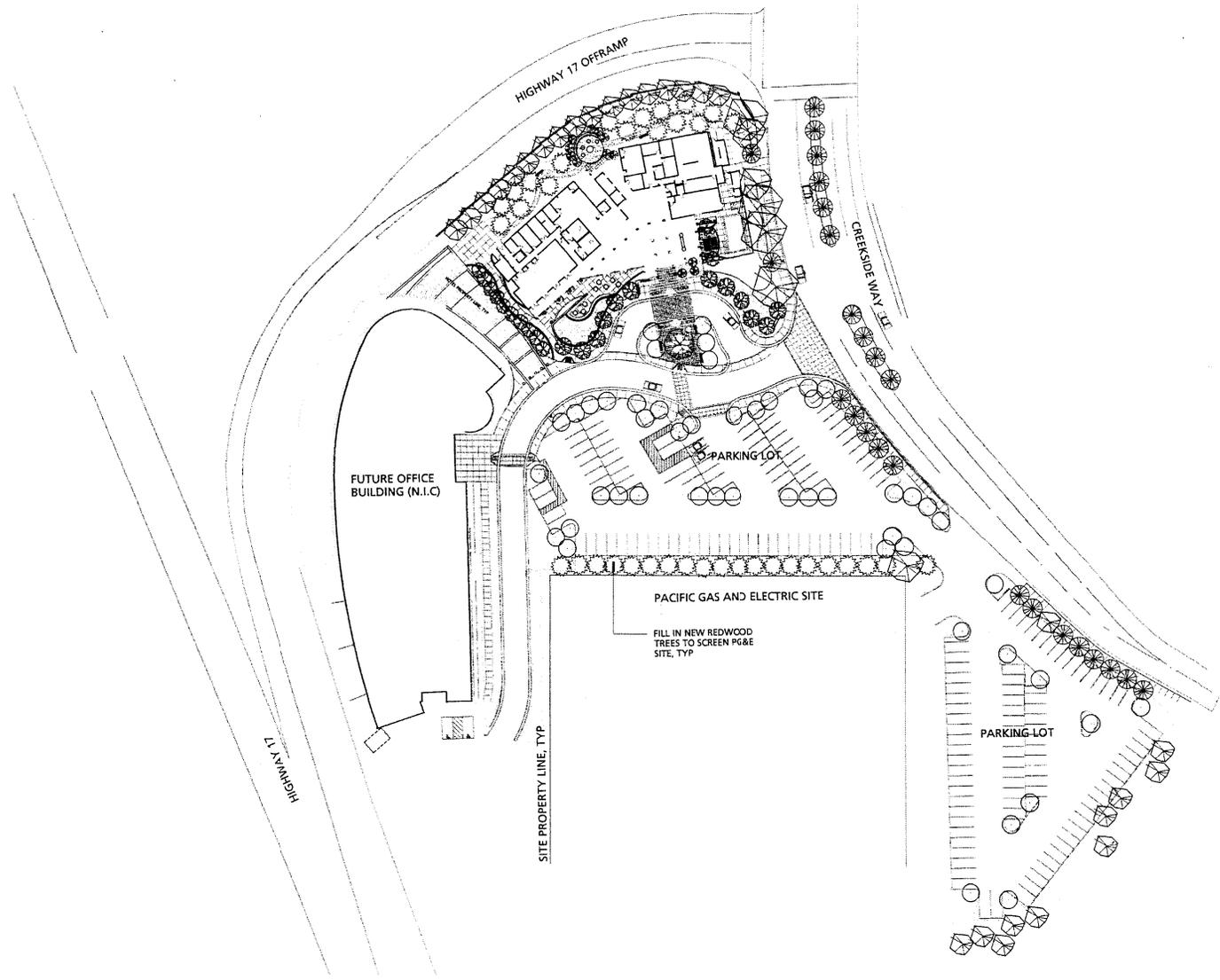
architect Steinberg Architects
 60 Pierce Avenue
 San Jose CA 95110

HILTON GARDEN INN
 CAMPBELL, CALIFORNIA

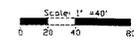
SHW-IL PROPERTY COMPANY
 30 East 4th Avenue
 SAN MATEO, CA 94401

STEINBERG ARCHITECTS
 project no: 05026 date: 04.10.2007

C1



CONCEPTUAL LANDSCAPE PLAN



ARCHITECT
 Stearnberg Architects
 60 Pierce Avenue
 San Jose CA 95110

HILTON GARDEN INN
 CAMPBELL, CALIFORNIA

SANDBL PROPERTY COMPANY
 30 East 4th Avenue
 San Mateo, Ca 94401



STYLANDERBARCHITECTS

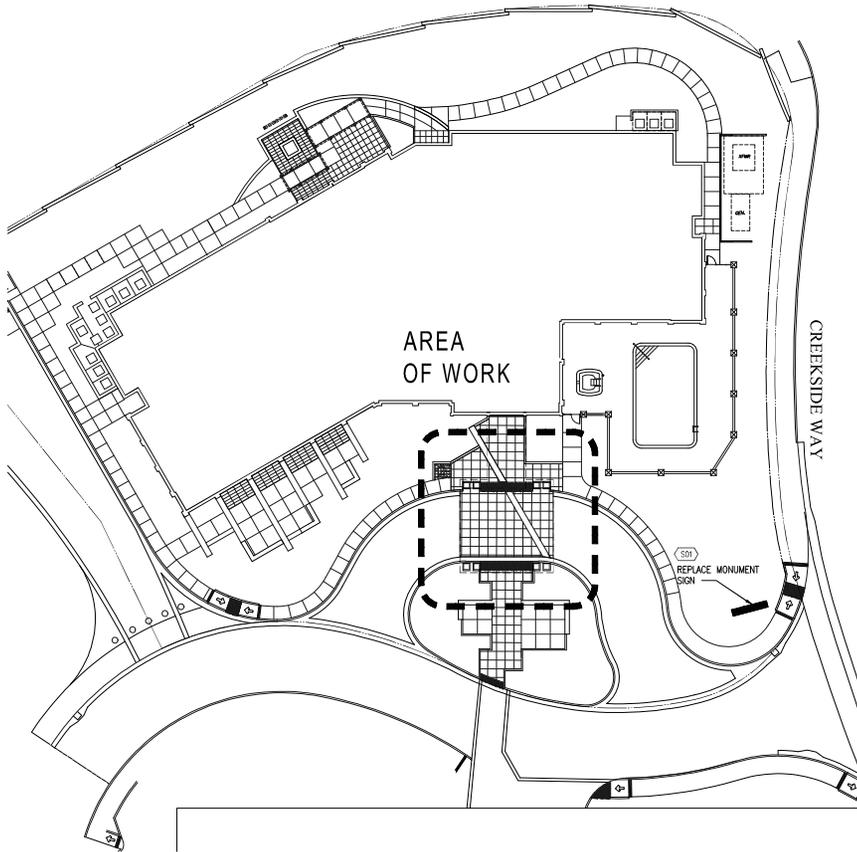
PROJECT NO: 05026 DATE: 04.09.07
THE GUZZARDO PARTNERSHIP INC.
 Landscape Architects - Land Planners
 836 Montgomery Street
 San Francisco, CA 94133
 T 415.483.4672
 F 415.423.5067

COURTYARD[®]

BY MARRIOTT



EXTERIOR PAINT + PORTICO MODIFICATION TO EXISTING HOTEL
655 Creekside Way | Campbell, CA 95008



CODE REVIEW

BUILDING WAS CONSTRUCTED IN 2008 UNDER THE 2007 CALIFORNIA BUILDING CODE.
BUILDING IS FULLY SPRINKLERED PER NFPA 13
NEW WORK SHALL COMPLY WITH THE THE FOLLOWING CODE AND ASSOCIATED CODES:
2022 CALIFORNIA BUILDING CODE AS ADOPTED BY THE CITY OF CAMPBELL

3. USE AND OCCUPANCY

- 3.1 NO CHANGE OF USE, MIXED USE, NON-SEPARATED
 - A-2 DINING / A-3 LOBBY
 - B OFFICES, LAUNDRY, STORAGE + MECHANICAL
 - R1 GUESTROOMS

4. SPECIAL REQUIREMENTS

- 4.1 NO CHANGES.

5. HEIGHT + AREA

- 5.1 NO AREA CHANGES - 55,473 s.f.
- 5.2 NO HEIGHT CHANGES. EXISTING BUILDING - 7 STORIES - 84'-0"

6. CONSTRUCTION TYPE

- 6.1 TYPE I-B (FULLY SPRINKLERED)

7. FIRE RESISTANCE

- 7.1 STRUCTURAL FRAME - 2 HRS
- 7.2 ROOF CONSTRUCTION - 1 HR

9. FIRE PROTECTION SYSTEMS

- 9.1 EXISTING OPERATING SPRINKLER SYSTEM REMAINS. MODIFICATIONS OR ADJUSTMENTS SHALL BE MADE BY A QUALIFIED CONTRACTOR/DESIGN. CONTRACTOR SHALL SUBMIT DEFERRED SUBMITTAL FOR THESE MODIFICATIONS

11. ACCESSIBILITY

- 11.1 SITE ACCESSIBILITY CURRENTLY PROVIDES THE FOLLOWING COMPLIANT FEATURES
 - DROP OFF
 - PATH OF TRAVEL TO ENTRY
 - ACCESSIBLE MAIN ENTRANCE
 - PATH OF TRAVEL TO EACH, AND WITHIN EACH AMENITY
- 11.2 ACCESSIBILITY IS ADDRESSED AS PART OF SEPARATE (INTERIOR) PERMIT

SCOPE

MAIN BUILDING

PAINTING
RESTORE ANY DAMAGED EXTERIOR FINISH
PREP AND PAINT ENTIRE BUILDING TO NEW COLORS.
PROVIDE NEW SEALANTS AND ACCESSORIES AS REQUIRED

SIGNAGE
REPLACE EXISTING CHANNEL LETTER SIGNS WITH NEW BRAND SIGNAGE
REPLACE DIRECTIONAL / ENTRY BOX SIGNS WITH NEW

PORTE COCHERE

REPLACE ROOF FRAMING, SOFFITS AND CEILING
REPLACE LIGHTING
REINSTALL SPRINKLERS
PROTECT AND REPAIR ALL ADJACENT MATERIALS AND SURFACES

CUPOLA (ALTERNATE)

REPLACE ROOF FRAMING AND CEILING
REINSTALL LIGHTING
REINSTALL SPRINKLERS
PROTECT AND REPAIR ALL ADJACENT MATERIALS AND SURFACES

PROJECT TEAM

ARCHITECT

COMPANY: JOSEPH M. MATTON, AIA
CONTACT: JOE MATTON
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PHONE: 206-880-2122
EMAIL: joe@mattonckdesignstudio.com

ENGINEER

COMPANY: PACIFIC ENGINEERING
CONTACT: MELISSA DOONAN, P.E. SE
ADDRESS: 9744 4TH AVE. NE, SUITE 300 | SEATTLE, WA 98115
PHONE: 206-323-8048 EXT 113
EMAIL: Mdoonan@pecenr.com

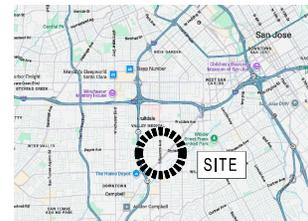
DRAWING INDEX

ARCHITECTURAL DRAWINGS	DATE
A000 COVER SHEET - SCOPE OF WORKS + GENERAL NOTES	5.18.2025
A001 MANDATORY MEASUREMENTS - CA GREEN	5.18.2025
A003 SPECIFICATIONS - PAINTING, EPS RESTORE + ROOFING	5.18.2025
A004 SPECIFICATIONS - FLASHING + SEALANTS	5.18.2025
A100 EXISTING SITE PLAN, NOTES AND DETAILS	5.30.2025
A000 BUILDING ELEVATIONS	5.18.2025
A001 ELEVATIONS - COLOR SELECTIONS	5.18.2025
A100 PORTE COCHERE DEMOLITION PLANS + SECTIONS	5.18.2025
A101 PORTE COCHERE CONSTRUCTION PLANS + SECTIONS	5.18.2025
A002 PORTE COCHERE ENLARGED SECTIONS	5.18.2025
A003 PORTE COCHERE ENLARGED DETAILS	5.18.2025

STRUCTURAL DRAWINGS	DATE
S1 GENERAL NOTES	5.8.2025
S2 FRAMING DETAILS	5.8.2025

REVISION LOG

VICINITY MAP



1 EXISTING SITE PLAN
NORTH
1" = 20' - 0"

COURTYARD BY MARRIOTT
CAMPBELL CALIFORNIA | HUNTINGTON HOTEL GROUP
2416 - MARSHA S.U.C.C.

COVER SHEET
AND NOTES
ISSUE DATE: 5.19.2025 | PERMIT

A E 0 0

AIR QUALITY TABLES

ADHESIVE VOC LIMIT ¹ Less Water and Less Exempt Compounds in Grams Per Liter	CURRENT VOC LIMIT
ARCHITECTURAL APPLICATIONS	
Indoor carpet adhesives	50
Carpet seal adhesives	50
Outdoor carpet adhesives	150
Wood flooring adhesive	100
Plastic floor adhesives	80
Subfloor adhesives	50
Concrete adhesives	65
VOC and exempt solids adhesives	50
Grout and joint adhesives	50
Core note adhesives	50
Multipurpose construction adhesives	70
Structural glazing adhesives	100
Single-ply roof membrane adhesives	250
Other adhesive not specifically listed	50
SPECIALTY APPLICATIONS	
PVC welding	510
CPVC welding	490
Adb welding	255
Plastic cement setting	250
Adhesive primer for plastics	550
Concrete adhesive	80
Specialty exterior contact adhesive	200
Structural wood member adhesive	140
Top and trim adhesive	250
SUBSTRATE SPECIFIC APPLICATIONS	
Metal to metal	30
Plastic to plastic	50
Plastic to metal (except wood)	80
Wood	50
Fiberglass	80

1. For adhesive to be used in a new building construction project the adhesive with the highest VOC content shall be limited.
2. For adhesive to be used in an existing building project to improve the VOC content specified in this table, see South Coast Air Quality Management District's (SCAQMD) website for more information.

SEALANT VOC LIMIT Less Water and Less Exempt Compounds in Grams per Liter	CURRENT VOC LIMIT
SEALANTS	
Architectural	250
Interior joint	750
Exterior window seal	300
Roofing	250
Single-ply roof membrane	450
Other	400
SEALANT PRIMERS	
Architectural	250
Roofing	775
Modified bituminous	500
Interior joint	750
Other	700

Note: For adhesive substrates regarding methods to reduce the VOC content specified in this table, see South Coast Air Quality Management District's (SCAQMD) website for more information.

VOC CONTENT LIMITS FOR ARCHITECTURAL COATINGS¹ Grams of VOC per Liter of Coating, Less Water and Less Exempt Compounds

COATING CATEGORY	CURRENT LIMIT
FINISH COATINGS	
Flat coatings	50
Nonflat coatings	100
Nonflat/flat glass coatings	150
SPECIALTY COATINGS	
Aluminum roof coatings	400
Blowdown roof coatings	50
Bituminous roof primers	350
Bond breakers	350
Concrete curing compounds	350
Concrete/resinous sealers	100
Driveway sealers	50
Epoxy coatings	180
Faux finishing coatings	350
Fire resistive coatings	50
Floor coatings	100
Ferrous release compounds	250
Graphite/carbon coatings (dry pigments)	50
High temperature coatings	>50
Industrial maintenance coatings	250
Low solids coatings ²	>50
Magnesium cement coatings	50
Metallic finish coatings	>50
Metallic pigmented coatings	50
Multicolor coatings	250
Protective metal primers	400
Primer, sealer, and undercoats	50
Reactive penetrating sealers	50
Resistive coatings	350
Roof coatings	50
Roof penetrative coatings	250
Sealants	300
Other	300
Epoxide	150
Specialty primers, sealers and undercoats	50
Spalls	250
Stone consolidants	50
Swimming pool coatings	40
Traffic wearing coatings	250
Top and trim finishes coatings	250
Waterproofing membranes	20
Wood coatings	250
Wood preservatives	250
Zinc-rich primers	40

1. Types of VOC are the following: aldehydes, ketones, and esters, except acetone.
2. This is a highly volatile solvent or other solvent blend that is an exempt substance as defined in Table 1.

CAL GREEN CHECKLIST

2022 CALGreen NONRESIDENTIAL MANDATORY MEASURES CHECKLIST

SECTION	REQUIREMENTS	SHEET	VERIFICATION	COMMENTS
5.1 PLANNING AND DESIGN (Site Development)				None apply
5.2 ENERGY EFFICIENCY				None apply
5.3 WATER EFFICIENCY AND CONSERVATION				None apply
5.4 MATERIAL CONSERVATION & RESOURCE EFFICIENCY (Construction Waste Reduction, Disposal & Recycling)				
5.504.1	Construction Waste Management Recycle and/or salvage for reuse a minimum of 65% of the non-hazardous construction waste in accordance with Section 408.1, 408.1.1, 408.1.2 or 408.1.3, or meet a local construction and demolition waste management ordinance, whichever is more stringent.	A02	Enforcing agency	Contractor shall complete records and be prepared to submit plans to authorities.
5.508.1.1	Construction Waste management plan Where a local jurisdiction does not have a construction waste management ordinance that is more stringent, submit a construction waste management plan that complies with items 1 through 4 of this section.	A02	Enforcing agency	See above
5.508.1.2	Waste management company Utilize a waste management company that can provide verifiable documentation that the percentage of construction and demolition waste material diverted from the landfill complies with this section.	A02	Enforcing agency	
5.508.1.4	Documentation Provide documentation of the waste management plan that meets the requirements listed in Sections 5.508.1.1 through 5.508.1.3, and the plan is accessible to the enforcement authority.	A02	Enforcing agency	
5.5 ENVIRONMENTAL QUALITY (Pollutant Control)				
5.504.1	Adhesives, Sealants and Coatings Adhesive and sealants used on the project shall meet the requirements of the following standards: 1. Adhesives, adhesive bonding primers, adhesive primers, sealers, sealant primers and sealers shall comply with local or regional pollution control or air quality management plans that are applicable to the project. 2. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 3. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 4. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 5. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 6. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 7. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 8. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 9. Adhesive and sealant shall be shown in Tables 5.504.1 and 5.504.1.2. 10. 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CAMPBELL WASTE MANAGEMENT

Campbell Waste Tracking Requirements



State building standards require 65% of debris generated by construction and demolition to be diverted from landfills. Failure to meet this requirement will result in fines and delays. Your project's debris diversion will be tracked with a Waste Management Plan (WMP) created through Green Halo at campbell.wastetracking.com.

- Step 1: Before You Begin**
- Starting March 1, 2024, Debris must be collected and disposed of in containers provided by a City-approved hauler(s). Current approved haulers are listed on page two of this handbook.
 - Tell the hauler you are tracking your debris generation for a WMP to receive disposal tickets.
 - To self-haul your debris, you must use your company's own disposal bins with your logo on it and take the debris to an approved facility listed on page two of this handbook. You may not use any other hauling or processing companies.
 - Create your WMP at campbell.wastetracking.com. Click [HERE](#) for a short video explaining how to use Green Halo, create your WMP, and upload debris tickets.
 - You will need to provide your permit (or permit application) number.
 - Create a separate WMP for each applicable permit. If you cannot separate the debris from multiple permits, you may use the same WMP but all permits must be reviewed and finalized at the same time.
 - If you have questions regarding how to use Green Halo, call their Customer Support at 1-888-525-1301.
- Step 2: Creating a WMP**
- Meeting a 65% Diversion Rate
 - Separate and sort your debris to your selected facility's material descriptions.
 - Debris with excessive amounts of wood will be classified as "wood or miscellaneous debris" and will lower your diversion rate.
 - Use Green Halo's "Recycler Search" to find facilities with high recycling rates.
 - Reuse, salvage, or donate debris or materials and upload relevant documentation to your WMP.
 - Only report debris from your permit's scope of work.
 - Prior to Final Review
 - Upload all disposal/recycling/donation tickets and reuse/salvage documentation
 - Submit your final WMP. City staff will be notified and review it.
- If you have questions about WMPs and/or Green Halo, call customer support at 1-888-525-1301. If you have questions about debris diversion policy and regulation, contact Port Newby at port@campbell.ca.gov.

1. Campbell Municipal Code 6.12.030
2. Campbell Municipal Code 6.12.040
3. Campbell Municipal Code 6.12.040



May 23, 2024
The West Valley Solid Waste Management Authority (Authority) and the cities of Campbell, Monte Sereno, and Saratoga; and the Town of Los Gatos (Member Agencies), require permitted construction and demolition (C&D) debris projects to utilize an approved C&D debris collection contractor or self-haul for transport of C&D debris within the Member Agencies' service area.

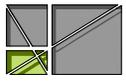
The use of an approved C&D processing facility is required for all mixed C&D debris. Source separated C&D debris shall be taken to any facility that can demonstrate minimum recovery rate of 90%. C&D debris intended for disposal shall be taken to the Guadalupe Landfill.

The use of any other C&D collection contractor or mixed processing facility outside of those listed below is prohibited.

Contractor Name	Address	Contact Information
Accurate Cleaning Systems (ACS)	PO Box 2465 Hollister, CA 95004	831-636-9767
Devcon Construction Inc.	690 Gibraltar Drive Milpitas, CA 95035	408-942-8200 www.devcon-constr.com
Eco Box Recycling	PO Box 630218 San Jose, CA 95161	408-639-1973 www.ecoboxrecycling.com
Ferma Greenbox Inc.	6647 Smith Ave Newark, CA 94560	877-337-6211 www.fermagreenbox.com
G+W Debris Services	675 Los Esteros Rd San Jose, CA 95134	408-538-4668 www.greenwaste.com/services/debris-boxes/bay-area/
Premier Recycle Company	348 Pheon Ave San Jose, CA 95112	408-297-7910 www.premierrecycle.com
Recology South Bay	1675 Rogers Ave San Jose, CA 95112	408-288-7200 www.recology.com
7 Days Box	1125 Maybury Rd San Jose, CA 95133	408-375-8670 www.iamrubbymasters.com
Valley Recycling	1615 S 7 th St #8 San Jose, CA 95112	408-297-5404 www.valleyservices.biz

Facility Name	Address	Contact Information
Guadalupe Recycling & Disposal	15999 Guadalupe Mines Rd San Jose, CA 95121	408-268-1670 www.guadalupe-wm.com
Leo Recycle	215 Leo Ave San Jose, CA 95112	408-780-5557 www.leorecycling.com
Premier Recycle Company	348 Pheon Ave San Jose, CA 95112	408-297-7910 www.premierrecycle.com
Superior Demolition Services, Inc.	12475 Lupton Ave San Martin, CA 95046	408-683-5654 www.superiordemolition.com
Valley Recycling	1615 S 7 th St #8 San Jose, CA 95112	408-297-5404 www.valleyservices.biz
Zanker Materials Processing Facility and Landfill	675 Los Esteros Rd San Jose, CA 95134	408-263-2384 www.greenwaste.com/facilities/san-jose-and-recycling/

1821 BASCOM AVE, #406 • CAMPBELL, CA 95008 • (408) 471-6255 • WESTVALLEYRECYCLES.COM



COURTYARD BY MARRIOTT
CAMPBELL CALIFORNIA | HUNTINGTON HOTEL GROUP
2416 - MARSHA SUCC

MANDATORY MEASURES
CALIFORNIA GREEN BUILDING CODE NOTES
ISSUE DATE: 5.19.2025 | PERMIT

A02

EIFS REHABILITATION SPEC

SECTION 07 24 00.1

Part 1 GENERAL
1.01 SUMMARY
 A. Section Includes:
 1. The replacement of existing Exterior Insulation and Finishing Systems Applications as described and specified herein including:
 a. Cleaning
 b. Repair
 c. Reinsulation
 2. Application of Substrate

1.02 REFERENCES
 A. EFS History Member Association (EFA) Publications
 1. EFA Guide for Use of Coatings with Exterior Insulation and Finishing Systems, Class FEP
 2. System Association (SA) Publications
 a. SA's "Recommended Specifications for the Application of Exterior Densified"
 3. International Code Council (ICC)
 D. Codes/Specs. Inc.
 1. Dval Document D2002 - Dval CARE System for Cleaning and Restoration of EFS
 2. Dval Document D2001 - EFS Inspection and Grading
 3. Dval Document D2003 - EFS Inspection and Grading
 4. Dval Document D2004 - EFS Inspection and Grading

1.03 DEFINITION
 A. EFS: Exterior Insulation and Finishing System
 B. Class FEP: Exterior Insulation and Finishing System (EIFS) as defined by ASTM C1363 practice for application of Class FEP Exterior Insulation and Finishing Systems with a "modified" substrate, exterior wall/ceiling system that consists of an insulation board attached adhesively to the substrate, a mineral wool reinforced base coat, and a surface protective finish coat.
 C. Surface Coating: Defined as a small surface area, spalls, or cracks that do not penetrate below the EFS base coat, and in which the EFS reinforcing material is not severed.
 D. Partial Surface Coating: Defined as any damaged flat surface the EFS reinforcing mesh and before the surface of the EFS board.
 E. Anchor: Any fastener that is used to secure or fasten the EFS into substrate. Minimum 50% of the installation board is required at the base of the joint. The fastener is to be secured using a minimum depth of the application of the EFS finish coat. It also serves to break up the necessity of a metal fastener to be installed into the substrate.
1.04 SYSTEM DESCRIPTION
 A. Design Requirements
 1. Evaluation and selection of existing EFS conditions
 a. Surface cracks
 b. Cracks which penetrate the EFS because it is over the EFS reinforcing mesh
 c. Porosity Damage
 d. Cracks at anchor points
 e. Sealed conditions in joints in EFS and between EFS and adjacent components such as different exterior finish materials, doors and windows.
 2. Development of schedule of repair methods for damage or defects to existing EFS
 3. System Methods: Repaired/Sealed protection coating 100% pure acrylic non-leak based materials. No material using non-acrylic resin in the form of seal or repair.
 4. Sealer System: Approved sealers are required at all exterior materials as specified in Section 07 20.00.
 B. Performance Requirements
 1. General: Provide system that complies with the following performance requirements:
 a. Bond strength: Free from bond failure within system components or between system and supporting wall structure, resulting from exposure to the wind, weather, or other environmental conditions.
 b. Water Penetration: Resistance to water penetration from exterior through the EFS system into interior of building from the installation of thermally-bridged elements or other degradation of system and assemblies including, but not limited to, supporting wall construction and interior finish installation.
 c. Moisture Control:
 1. Prevent accumulation of water behind the EFS system, either by condensation or leakage through the substrate, in the design and detailing of the wall assembly.
 2. Provide flashing to extend water behind a flange to penetrate completely in the wall assembly, including, above, alongside and under door, sashes, window and door sill, without water penetration, direct, adjacent to lower walls with higher walls, above, alongside and under door, and the like of the wall.
 3. All Leakage Prevention: Provide protection of a barrier system in foundation root, window and door, and other penetrations through the system with connecting and compatible materials consistent to systems construction and leakage sealing as required.
1.05 QUALITY ASSURANCE
 A. Compliance:
 1. The manufacturer shall notify a copy of their approved anchor certificate issued by the EFS Association.
 2. The Inspection Board Member shall approve the EFS manufacturer or verify the EFS manufacturer. Attach this letter to warranty.
 3. The manufacturer shall be a member of the Exterior Insulation and Finishing Association - EFA.
 4. The installer shall have a minimum of five years experience installing the specified product in projects similar in scope, and with a record of successful performance.
 B. The EFS installer shall provide the Submittals as acceptable prior to application of the receiving system.
 C. The system specified includes the following materials and components:
 1. EFS History Member Association (EFA) Publications
 2. System Association (SA) Publications
 3. International Code Council (ICC)
 D. Codes/Specs. Inc.
1.06 PROJECT CONDITIONS
 A. Environmental Limitations: Do not install system when ambient outdoor air or any substrate temperature are 40 deg F and falling unless temporary protection will be provided to maintain minimum temperature above 40 deg F and prevent damage to work and materials and allow time for curing and moisture resistant film for at least 24 hours after installation.
 B. Adhesive application to wall system shall be performed during installation, while curing and/or unattended, from weather and other damaging conditions.
 C. Do not apply material in direct sun.
1.07 COORDINATION
 A. The work of this Section shall be coordinated with the work specified in related Sections. This includes installation of new EFS system, repair of other exterior building components such as windows, doors, and other penetrating devices, and detailing and installing of existing EFS system. Outstanding issues and concerns shall be resolved at the pre-construction meeting prior to start of installation.
 B. Temporary protection shall be provided to the work specified in this section to protect the installation of the work and finishing systems of all locations that could be affected by the work.
 C. Do not allow water to penetrate below the EFS system.
 D. EFS shall be applied to maintain open area of the wall.
1.08 WARRANTY
 A. EFS shall be installed shall be free from material and/or application of the applied system.
 B. Provide a written seal application after 2 years of EFS repair as permitted by manufacturer's recommendations.
1.09 MAINTENANCE
 A. Maintenance Job:
 1. Clean up materials 10' and over in size from site directed by Owner's Representative.
 2. Contents of bins shall not be used.
 3. Protect appearance of surfaces:
 a. One gallon of surface
 b. Three quart appearance of surfaces
 c. One gallon of fresh color coating for each color used
 d. Two quart appearance of surfaces
 e. Two quart of insulation board
1.10 WARRANTY
 A. Manufacturer shall provide written warranties on the labor and materials associated with the EFS rehabilitation system. This warranty is exclusive of EFS.
 1. Manufacturer shall provide a 15-year material warranty on receiving system.
 2. Manufacturer shall provide a 5-year material warranty on all repair material specified herein and in the Manufacturer's repair guidelines.
 3. This warranty shall provide a 2-year warranty for all workmanship related to EFS application.
 B. Work is warranted against:
 1. Material defects, including, but not limited to, peeling, cracking, delamination, flaking, or surface failures.
 2. Shrinkage and bulging of the EFS member installed according to the EFS in the following or similar applications through the system, EFS in EFS and EFS in dissimilar substrates.
 3. Insects.
 4. The installer must have an inspection by manufacturer's representatives including EFS, Sealer and Coatings. Inspection reports shall be included with warranty documents including final inspection and written verification at the completion of application of the system, including all ongoing safety items.
Part 2 PRODUCTS
2.01 MANUFACTURERS
 A. EFS History Member Association (EFA) Publications
 1. Dval System, Inc., 401-842-1180
 a. Contact: Bob Stone, 10076 North 36th Street (770-331-3420)
 b. E-File Code
 2. Dval System, Inc. (Canada/US), 401-842-1180
 3. Dval System, Inc. (USA/Canada), 401-842-1180
 4. Contact: Dval System, Inc.
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FLASHING

SHEET METAL + TRIM

SECTION 07 52 00

Part 1 GENERAL

1.01 SUMMARY

- A. Section Includes:
 1. Flashing at Roof Windows, Doors, and other Locations as shown on Drawings.
 2. Aluminum Formed Cap Flashings (FCFs)
 3. Flashings
- B. Related Sections:
 1. Section 07 54 00 (2740) - Exterior Insulation and Finish Systems (EIFS)
 2. Section 07 52 01 (0752) - Electronic Weathering Flashing
 3. Section 07 52 02 (0752) - Joint Sealants

1.02 REFERENCES

- A. ASTM International Publications:
 1. "ASTM Standard Specification for Sealed Metal"
 2. "B308 Standard Specification for Aluminum and Aluminum-Alloy Sheet and Plate"
 3. "C933 Standard Specification for Extruded Joint Sealant"
 4. "D3379 Standard Test Method for Testing Waterproofing and Deterioration Materials"
- B. American Architectural Manufacturers Association (AAMA) Publications:
 1. "A140 Standard Specification for Aluminum Siding, Soffit and Fascia"
 2. "Sheet Metal and Air Conditioning Contractors National Association (SMACNA) Publications:
 - a. "Architectural Sheet Metal Manual" Current Edition
 3. Single-Ply Roofing Systems (SPRS) Publications
 4. "ANSI/SPR-500 "Wind-Driven Standard for Edge Systems Used with Low-Slope Roofing Systems"
 5. The Society for Protective Coatings (SPC) Publications
 1. "SSPC Painting Manual"
 2. National Association of Architectural Metal Manufacturers (NAAMM) Publications:
 1. "Metal Finishes Manual"

1.03 PERFORMANCE REQUIREMENTS

- A. General: Flash sheet metal flashing and trim to withstand wind loads, structural movement, thermal induced movement, and exposure to weather without failure.
- B. Flashing and trim shall be designed, tested for, and installed in accordance with Chapter 10 of the International Building Code (IBC) and refer to resistance in accordance with AIA/CES/IBCF 150.
- C. Thermal Movements: Provide sheet metal flashing and trim that allow for thermal movements including the following manner of contact and surface interaction by preventing blocking, wedging of joints, the absorption, accumulation or compression, failure of joint sealants, failure of connections and other detrimental effects. Provide flash trim that resist rotation and avoid stress areas as a result of thermal movement, thermal expansion/contraction, 150 Degree Fahrenheit (65 Degree Celsius) temperature change.
- D. Wind Uplift: Provide sheet metal flashing and trim that will resist wind uplift vibration in building interior.

1.04 SUBMITTALS

- A. General: Submit the following in accordance with Conditions of Contract and Division 01 Specification Sections.
 1. Product Data: Submit "Letter of Confirmation" indicating specific items used in project with the following supporting data:
 1. Manufacturer's standard options chart for selection purposes.
- B. QUALITY ASSURANCE:
 1. Insurable Qualifications: Engage an experienced contractor who has completed sheet metal flashing and trim work similar in material, design, and extent to that indicated on this project and with a record of successful installation performance.
 2. Do not proceed with the installation of flashing and sheet metal work until submittal construction, cast site, flashing, repair, and other construction to resolve the work is completed.
 3. Flashing and sheet metal shall be installed in accordance with:
 1. FM Global (FMCS) Standards (Factory Mutual Engineering and Research Requirements)
 2. SMACNA Architectural Sheet Metal Manual
 3. Current US standards and profiles shown unless other requirements are indicated.
 4. Do not include: metal expansion and contraction and other details not to be performed, and only the Architect in writing of unambiguous direction. Do not proceed with work until satisfactory conditions have been reached in a manner acceptable to the architect.

1.05 PROJECT CONDITIONS

- A. Existing Construction Conditions:
 1. Exercise care when working on or about roof surfaces to avoid damaging or disturbing existing flashing.
 2. Place approved work on roof surfaces adjacent to work of this Section and on access routes. Keep in place and maintain during work.
 3. DO NOT WALK ON THE TOP OF ROOF MEMBRANE UNLESS NECESSARY TO COMPLETE WORK.
- B. Roofing and flashing shall not be subject during installation and shall not be stained in the event there is a deterioration or penetration of work. Metal flashing material be subject when ambient temperature is below 35 degrees F.
- C. Coordinate work of this Section with waterproofing and waterproofing flash to proper installation of each installation. Ensure best possible weather resistance, durability of work, and protection of materials and finishes.

1.07 DELIVERY, STORAGE, AND HANDLING

- A. Delivery: Deliver materials in manufacturer's original, unopened, undamaged containers with identification labels only.
- B. Storage and Protection: Protect materials from rain and physical damage. Provide cover on and on all sides, allowing for adequate ventilation. Store flashing sheets horizontally with care to avoid damage. Store products in a dry area away from high heat, flames or gases.
- C. Store Membrane Flashing System products in manufacturer's unopened packaging until ready for installation and dispose the correct amounts of materials from the manufacturer's use.

1.08 WARRANTY

- A. Provide Owner with warranty stating that flashing material will properly shed water and protect roof and wall from physical damage for a minimum period of 50 years from date of substantial Completion and the damage resulting from failure to provide above stated performance will be repaired to satisfaction of Owner at no additional cost.

Part 2 PRODUCTS

- A. ALUMINUM FLASHING AND SHEET METAL:
 1. Material:
 - a. Aluminum Sheet: ASTM B308, Alloy 3003, Temper H14, BA-C22041, minimum .022 inch thick (20 gsm sheet).
 - b. Flash: Single-Flashing "Seal System" Manufacturer's standard Single-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
 - c. Flashings: Consider type of same material as flashing; sized to suit application.
 - d. Size and shape as shown on Drawings.
 - e. Color: Refer to General Product Data.
- B. ALUMINUM FORMED CAP FLASHING:
 1. Approved Manufacturers:
 - a. "Therma-Cap Flashing" (Therma-Cap) Model E-11, (800)545-1102
 2. "Permaseal Caping" (W.P. Holman Company) (800)314-5463
 3. "Therma-Cap Flashing" (Permaseal Systems, a division of Southern Aluminum Finishing Company, Inc.) (800)340-8823
 2. Aluminum Formed Cap Flashing shall be smooth Aluminum, 3003 H14 temper, with seal and shape as shown on Drawings. All joints shall receive specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
 3. Finish: Single-Flashing "Seal System" Manufacturer's standard Single-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
 4. Color as shown on Exterior Flashings.
 5. Flashings: Consider type: Sizes 100 mils/min maximum and wetters; sized to suit application.
 6. Truncated Flashing:
 1. Approved Manufacturers:
 - a. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
 2. "Therma-Cap Flashing" (Permaseal Systems, a division of Southern Aluminum Finishing Company, Inc.) (800)340-8823
 3. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
 4. "Therma-Cap Flashing" (Permaseal Systems, a division of Southern Aluminum Finishing Company, Inc.) (800)340-8823
 5. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.

- B. METALLIC GLAZING MATERIALS AND ACCESSORIES:
 1. Flashings: Flash sheet metal flashing and trim that allow for thermal movements including the following manner of contact and surface interaction by preventing blocking, wedging of joints, the absorption, accumulation or compression, failure of joint sealants, failure of connections and other detrimental effects. Provide flash trim that resist rotation and avoid stress areas as a result of thermal movement, thermal expansion/contraction, 150 Degree Fahrenheit (65 Degree Celsius) temperature change.
 2. Flashing and flashing shall not be subject during installation and shall not be stained in the event there is a deterioration or penetration of work. Metal flashing material be subject when ambient temperature is below 35 degrees F.
 3. Coordinate work of this Section with waterproofing and waterproofing flash to proper installation of each installation. Ensure best possible weather resistance, durability of work, and protection of materials and finishes.
- C. Easy Snap Sealant: Two-part, non-solvent, aluminum sealant-composite compound, recommended by aluminum manufacturer for exterior and interior non-porous joints, including metal-to-metal.
- D. Adhesive: Type recommended by flashing sheet metal manufacturer for waterproof and weather-resistant sealing and adhesive application of flashing sheet metal.
- E. Metal Sealants: Provide sheet metal, fill, stop, anchoring devices, and other accessory items as required for installation of work, matching or complying with flashing being installed; non-solvent, zinc and thickness required to performance.

END OF SECTION

JOINT SEALANTS

SPCC

SECTION 07 52 00

Part 1 GENERAL

1.01 SUMMARY

- A. Section Includes:
 1. Clean and Prepare Joint Surfaces
 2. Sealant and Backing Material including primers, backer rods, bond breakers and accessories.
- B. Related Sections:
 1. Section 04 70 00 - Manufacturing Machinery
 2. Section 07 02 00 (0702) - Sheet Metal Flashing and Trim
 3. Section 09 00 00 - Doors
 4. Section 09 00 00 (0900) - Parting

1.02 REFERENCES

- A. ASTM International Publications:
 1. "C933 Standard Specification for Latex Sealants"
 2. "C922 Standard Specification for Elastomer Joint Sealants"
 3. "C1183 Standard Guide for Use of Joint Sealants"
 4. "C1248 Standard Test Method for Staining of Porous Substrate by Joint Sealant"
- B. ASTM D1915 Standard Specification for Flexible Cellular Materials - Sponge or Expanded Rubber
- C. ASTM D1915 Standard Specification for Flexible Cellular Materials - Sponge or Expanded Rubber
- D. ASTM D1915 Standard Specification for Flexible Cellular Materials - Sponge or Expanded Rubber
- E. "ASTM D1915 Standard Specification for Flexible Cellular Materials - Sponge or Expanded Rubber"
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- Z. "ASTM D1915 Standard Specification for Flexible Cellular Materials - Sponge or Expanded Rubber"

1.03 PERFORMANCE REQUIREMENTS

- A. General: Flash sheet metal flashing and trim to withstand wind loads, structural movement, thermal induced movement, and exposure to weather without failure.
- B. Flashing and trim shall be designed, tested for, and installed in accordance with Chapter 10 of the International Building Code (IBC) and refer to resistance in accordance with AIA/CES/IBCF 150.
- C. Thermal Movements: Provide sheet metal flashing and trim that allow for thermal movements including the following manner of contact and surface interaction by preventing blocking, wedging of joints, the absorption, accumulation or compression, failure of joint sealants, failure of connections and other detrimental effects. Provide flash trim that resist rotation and avoid stress areas as a result of thermal movement, thermal expansion/contraction, 150 Degree Fahrenheit (65 Degree Celsius) temperature change.
- D. Wind Uplift: Provide sheet metal flashing and trim that will resist wind uplift vibration in building interior.

1.04 SUBMITTALS

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 1. Product Data: Submit "Letter of Confirmation" indicating specific items used in project with the following supporting data:
 1. Manufacturer's standard options chart for selection purposes.
- B. QUALITY ASSURANCE:
 1. Insurable Qualifications: Engage an experienced contractor who has completed sheet metal flashing and trim work similar in material, design, and extent to that indicated on this project and with a record of successful installation performance.
 2. Do not proceed with the installation of flashing and sheet metal work until submittal construction, cast site, flashing, repair, and other construction to resolve the work is completed.
 3. Flashing and sheet metal shall be installed in accordance with:
 1. FM Global (FMCS) Standards (Factory Mutual Engineering and Research Requirements)
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 4. Do not include: metal expansion and contraction and other details not to be performed, and only the Architect in writing of unambiguous direction. Do not proceed with work until satisfactory conditions have been reached in a manner acceptable to the architect.

1.05 PROJECT CONDITIONS

- A. Existing Construction Conditions:
 1. Exercise care when working on or about roof surfaces to avoid damaging or disturbing existing flashing.
 2. Place approved work on roof surfaces adjacent to work of this Section and on access routes. Keep in place and maintain during work.
 3. DO NOT WALK ON THE TOP OF ROOF MEMBRANE UNLESS NECESSARY TO COMPLETE WORK.
- B. Roofing and flashing shall not be subject during installation and shall not be stained in the event there is a deterioration or penetration of work. Metal flashing material be subject when ambient temperature is below 35 degrees F.
- C. Coordinate work of this Section with waterproofing and waterproofing flash to proper installation of each installation. Ensure best possible weather resistance, durability of work, and protection of materials and finishes.

1.07 DELIVERY, STORAGE, AND HANDLING

- A. Delivery: Deliver materials in manufacturer's original, unopened, undamaged containers with identification labels only.
- B. Storage and Protection: Protect materials from rain and physical damage. Provide cover on and on all sides, allowing for adequate ventilation. Store flashing sheets horizontally with care to avoid damage. Store products in a dry area away from high heat, flames or gases.
- C. Store Membrane Flashing System products in manufacturer's unopened packaging until ready for installation and dispose the correct amounts of materials from the manufacturer's use.

1.08 WARRANTY

- A. Provide Owner with warranty stating that flashing material will properly shed water and protect roof and wall from physical damage for a minimum period of 50 years from date of substantial Completion and the damage resulting from failure to provide above stated performance will be repaired to satisfaction of Owner at no additional cost.

Part 2 PRODUCTS

- A. ALUMINUM FLASHING AND SHEET METAL:
 1. Material:
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 - d. Size and shape as shown on Drawings.
 - e. Color: Refer to General Product Data.
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 5. Flashings: Consider type: Sizes 100 mils/min maximum and wetters; sized to suit application.
 6. Truncated Flashing:
 1. Approved Manufacturers:
 - a. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
 2. "Therma-Cap Flashing" (Permaseal Systems, a division of Southern Aluminum Finishing Company, Inc.) (800)340-8823
 3. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
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- B. METALLIC GLAZING MATERIALS AND ACCESSORIES:
 1. Flashings: Flash sheet metal flashing and trim that allow for thermal movements including the following manner of contact and surface interaction by preventing blocking, wedging of joints, the absorption, accumulation or compression, failure of joint sealants, failure of connections and other detrimental effects. Provide flash trim that resist rotation and avoid stress areas as a result of thermal movement, thermal expansion/contraction, 150 Degree Fahrenheit (65 Degree Celsius) temperature change.
 2. Flashing and flashing shall not be subject during installation and shall not be stained in the event there is a deterioration or penetration of work. Metal flashing material be subject when ambient temperature is below 35 degrees F.
 3. Coordinate work of this Section with waterproofing and waterproofing flash to proper installation of each installation. Ensure best possible weather resistance, durability of work, and protection of materials and finishes.
- C. Easy Snap Sealant: Two-part, non-solvent, aluminum sealant-composite compound, recommended by aluminum manufacturer for exterior and interior non-porous joints, including metal-to-metal.
- D. Adhesive: Type recommended by flashing sheet metal manufacturer for waterproof and weather-resistant sealing and adhesive application of flashing sheet metal.
- E. Metal Sealants: Provide sheet metal, fill, stop, anchoring devices, and other accessory items as required for installation of work, matching or complying with flashing being installed; non-solvent, zinc and thickness required to performance.

END OF SECTION

Part 2 PRODUCTS-CONTD

- 2. Acceptable Products:
 - a. "Dynamar III" (Peters Corp.) (800)242-6888
 - b. "Dynamar III" (Peters Corp.) (800)242-6888
 - c. "Dynamar III" (Peters Corp.) (800)242-6888
 - d. "Dynamar III" (Peters Corp.) (800)242-6888
 - e. "Dynamar III" (Peters Corp.) (800)242-6888
 - f. "Dynamar III" (Peters Corp.) (800)242-6888
 - g. "Dynamar III" (Peters Corp.) (800)242-6888
 - h. "Dynamar III" (Peters Corp.) (800)242-6888
 - i. "Dynamar III" (Peters Corp.) (800)242-6888
 - j. "Dynamar III" (Peters Corp.) (800)242-6888
 - k. "Dynamar III" (Peters Corp.) (800)242-6888
 - l. "Dynamar III" (Peters Corp.) (800)242-6888
 - m. "Dynamar III" (Peters Corp.) (800)242-6888
 - n. "Dynamar III" (Peters Corp.) (800)242-6888
 - o. "Dynamar III" (Peters Corp.) (800)242-6888
 - p. "Dynamar III" (Peters Corp.) (800)242-6888
 - q. "Dynamar III" (Peters Corp.) (800)242-6888
 - r. "Dynamar III" (Peters Corp.) (800)242-6888
 - s. "Dynamar III" (Peters Corp.) (800)242-6888
 - t. "Dynamar III" (Peters Corp.) (800)242-6888
 - u. "Dynamar III" (Peters Corp.) (800)242-6888
 - v. "Dynamar III" (Peters Corp.) (800)242-6888
 - w. "Dynamar III" (Peters Corp.) (800)242-6888
 - x. "Dynamar III" (Peters Corp.) (800)242-6888
 - y. "Dynamar III" (Peters Corp.) (800)242-6888
 - z. "Dynamar III" (Peters Corp.) (800)242-6888

2.03 ACCESSORIES

- D. Primer: Non-solvent, non-solvent, recommended by sealant manufacturer to suit application.
- E. Joint Sealer: Non-solvent and non-solvent type, recommended by sealant manufacturer, compatible with joint forming materials.
- F. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- G. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- H. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- I. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- J. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- K. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- L. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- M. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- N. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- O. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
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- S. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- T. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- U. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- V. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- W. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- X. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- Y. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.
- Z. Joint Filler: Non-solvent, recommended by sealant manufacturer to suit application.

2.04 APPROVALS

- A. Approval:
 1. "Therma-Cap Flashing" (Therma-Cap) Model E-11, (800)545-1102
 2. "Permaseal Caping" (W.P. Holman Company) (800)314-5463
 3. "Therma-Cap Flashing" (Permaseal Systems, a division of Southern Aluminum Finishing Company, Inc.) (800)340-8823
 4. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat containing not less than 70 percent polyethylene flastic resin by weight, complying with AAMA 2604 where required.
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 44. "Eco-Flash" (Eco-Flashing) "Seal System" Manufacturer's standard Eco-Flashing system consisting of specially formulated adhesive primer and fluoropolymer color topcoat



SITE NOTES

- SITE PLAN IS FOR REFERENCE ONLY
- ORIGINAL PARKING PROHIBED (PRIOR TO GARAGE) = 177 SPACES
 - 171 STANDARD SPACES
 - 6 ACCESSIBLE SPACES
 - 1 VAN ACCESSIBLE SPACE
- REFER TO OWNERS SCOPE OF WORK, EXTERIOR PAINTING / ALL EXTERIOR WORK MAY BE INCLUDED IN SEPARATE PROJECT.

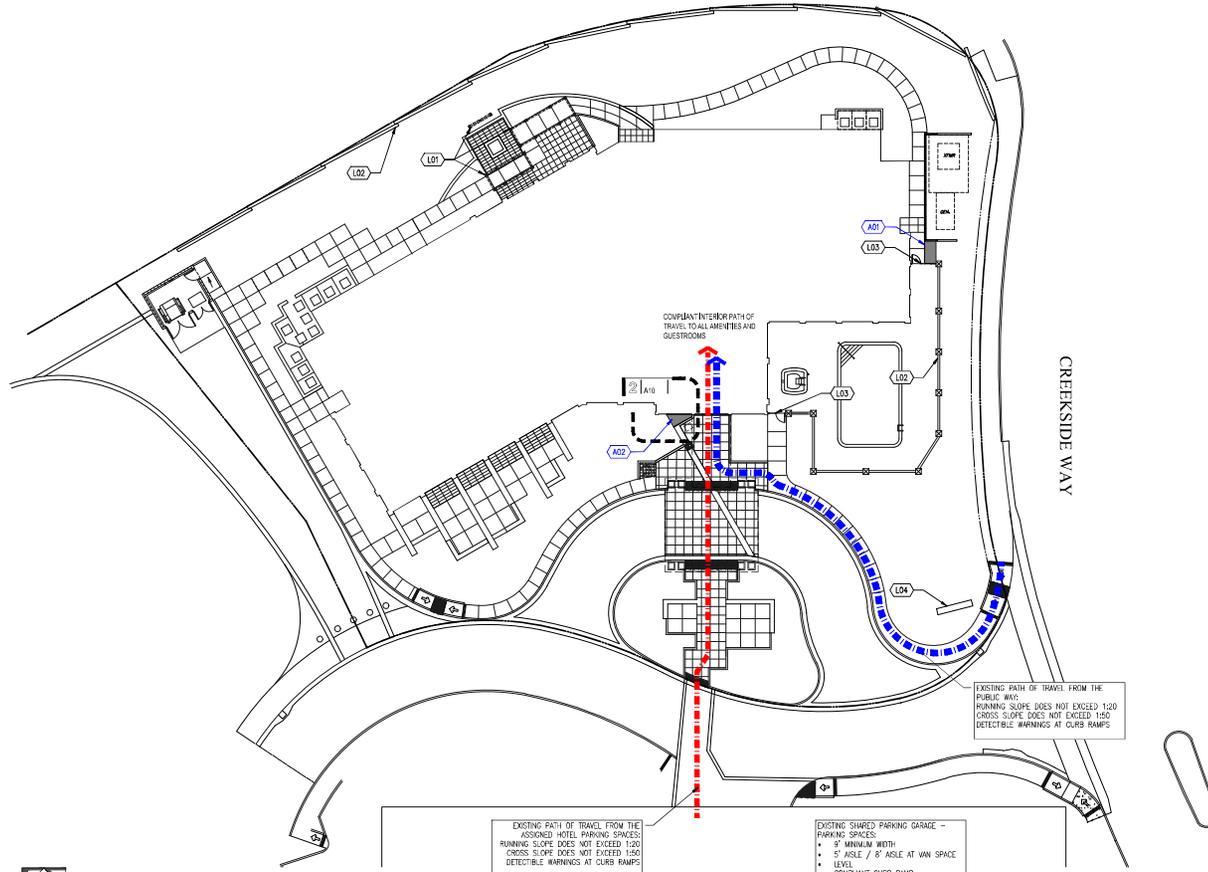
EXTERIOR NOTES

LANDSCAPE NOTES

- SAND PREP AND GRADE STAIN EXTERIOR TRIELUS. ALLOW FOR REPLACEMENT OF 5% OF FENCES AND CLIPSHARDWARE.
- PRESSURE WASH AND PAINT PREVIOUSLY PAINTED PRIVACY WALLS AND SCREENS - TYPICAL. PROTECT BRICK / STONE SURFACES
- PREP AND PAINT STEEL GATES AND FENCES TYPICAL
- COORDINATE REPLACEMENT OF MONUMENT SIGNS WITH OWNERS SIGN CONTRACTOR.

SITE ACCESSIBILITY

- INSTALL NEW PAVING TO ENSURE 24" PULL-SIDE CLEARANCE AT POOL GATE. SEE PHOTO MARKUP THIS SHEET
- RELOCATE TRASH RECEPTACLE AND EXTEND PAVING TO PROVIDE ACCESS TO CARD READER AND DOOR BUZZER.

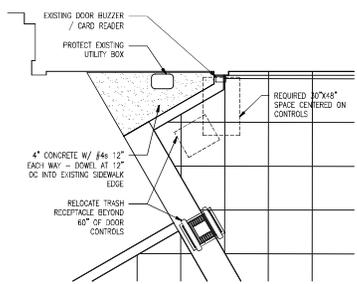


EXISTING PATH OF TRAVEL FROM THE ASSIGNED HOTEL PARKING SPACES:
 FINISHING SLOPE DOES NOT EXCEED 1:20
 CROSS SLOPE DOES NOT EXCEED 1:50
 DETECTIBLE WARNINGS AT CURB RAMP

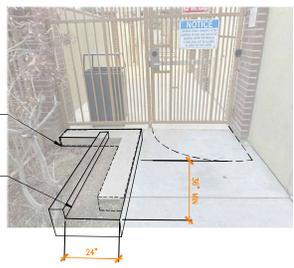
EXISTING SHARED PARKING GARAGE - PARKING SPACES:
 • 9" MINIMUM WIDTH
 • 5" AISLE / 8" AISLE AT VAN SPACE
 • LEVEL
 • COMPLIANT CURB RAMP

EXISTING PATH OF TRAVEL FROM THE PUBLIC WAY:
 FINISHING SLOPE DOES NOT EXCEED 1:20
 CROSS SLOPE DOES NOT EXCEED 1:50
 DETECTIBLE WARNINGS AT CURB RAMP

1 EXISTING SITE PLAN
 1" = 20'-0"



2 MAIN ENTRY MODIFICATION
 1/4" = 1'-0"



3 POOL GATE MODIFICATION
 NOT TO SCALE

COURTYARD BY MARRIOTT
 CAMPBELL CALIFORNIA | HUNTINGTON HOTEL GROUP
 2416 - MARSHA S.L.C.C.

EXISTING SITE PLAN
 NOTES AND DETAILS
 ISSUE DATE: 5 . 30 . 2025 | 100% PERMIT

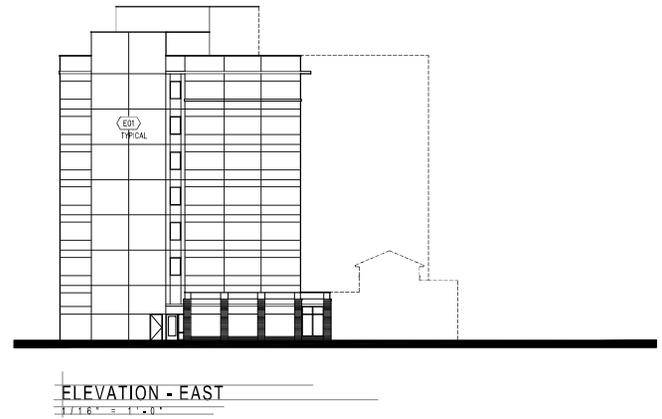
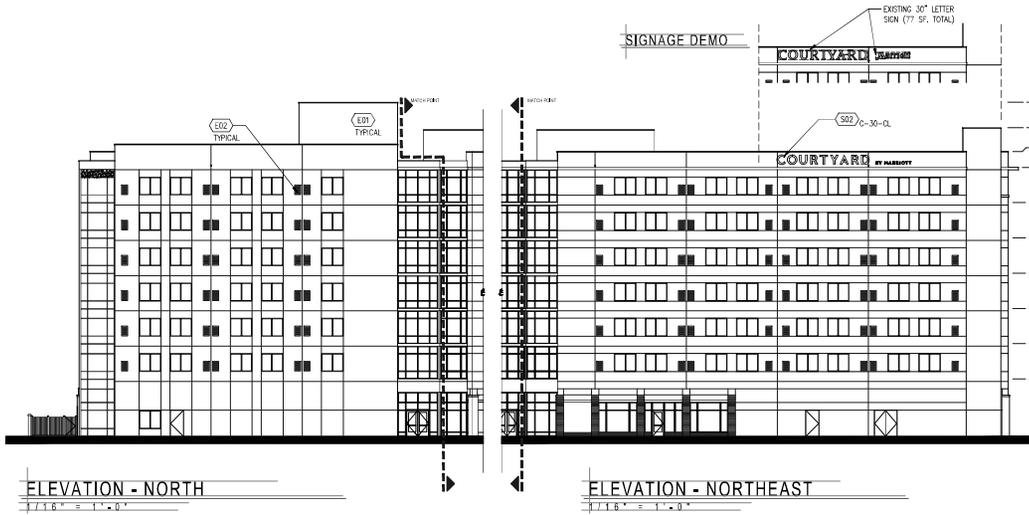
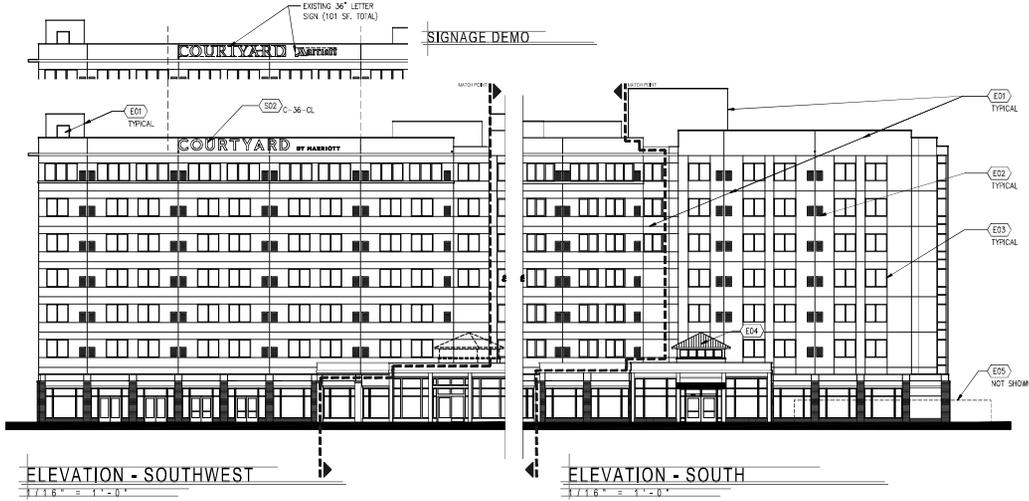
A10



COURTYARD BY MARRIOTT
 BUILDING ELEVATIONS AND NOTES
 2416 - MARSHA S.U.C.C.

BUILDING ELEVATIONS
 ISSUE DATE: 5.19.2025 | PERMIT

A90



SHEET NOTES

- NON-PAINING TO INCLUDE ALL BUILDING AREAS INCLUDING THOSE HIDDEN OR NOT OTHERWISE SHOWN IN THIS SET.
- DOWNSPUTS AND GRILLS TO BE PAINTED ARE NOT SHOWN TYPICALLY, BUT CONTRACTORS RESPONSIBLE FOR QUANTIFYING AND PAINTING THESE ITEMS.
- ADVISE OWNER OF ANY AREAS OF OBSERVE PRIOR TO PAINTING. VERIFY QUANTITIES OF REPAIR WORK WITH PROJECT MANAGER PRIOR TO ACCEPTANCE OF BBS.

SIGNAGE

- SIGNAGE DESIGNATIONS ARE BASED ON MARRIOTT'S EXTERIOR SIGNAGE MANUAL C-36-CL = 36" HIGH MAIN LETTERS (COURTYARD) 11/2" SECONDARY LETTERS (BY MARRIOTT) C-36-CL = 36" HIGH MAIN LETTERS (COURTYARD) 8" SECONDARY LETTERS (BY MARRIOTT)
- SIGNS ARE PROVIDED BY OWNER FROM APPROVED SIGNAGE VENDORS. CONTRACTOR TO COORDINATE NECESSARY MOUNTING AND ELECTRICAL ADJUSTMENTS (IF REQUIRED)
- SIGN AREA IS LESS THAN THE EXISTING SIGNS.
 CURRENT SOUTHWEST SIGN - 101 S.F. PROPOSED SIGN - 102 S.F.
 CURRENT NORTHEAST SIGN - 77 S.F. PROPOSED SIGN - 71 S.F.
- MONUMENT SIGN BOX TO BE REPLACED WITH THE LARGEST UNIT THAT WILL FIT WITHIN THE EXISTING FOOTPRINT. SIGNAGE CONTRACTOR TO VERIFY FINISH SIDE AND SELECT APPROPRIATE MOUNTING BOX.
- CHANNEL LETTERS SHALL APPEAR WHITE (ON DARK BACKGROUND) DURING THE DAY AND ILLUMINATE WHITE IN THE EVENING.
- SIGN PRIME PAINT BEING PULLED BY INSTALLER MANUFACTURER.**

Building Channel Letters

Channel Letter Specifications:
 180° maximum channel width (400° channel depth)
 18 S.D.A. 1.40117 3.2 2.0 www.uschannel.com Channel letters
 310° 1200 white acrylic/tempered glass acrylic/tempered glass
 1" post to match Postech/Chelco 400 C-grids/400/400, 400 1/2"
 180° max. depth
 1/2" to 1/4" (1/8" depth) 1/2" to 1/4" (1/8" depth)
 1/2" to 1/4" (1/8" depth) 1/2" to 1/4" (1/8" depth)
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 1/2" to 1/4" (1/8" depth) 1/2" to 1/4" (1/8" depth)
 Channel Letter Face Specifications:
 310° 1200 white acrylic/tempered glass acrylic/tempered glass

ELEVATION NOTES

- ELEVATIONS**
- PREP AND PAINT EXTERIOR INCLUDING EIFS, CONCRETE, WOOD TRIM AND ALL PREVIOUSLY PAINTED ITEMS - SEE COLOR ELEVATIONS, COLOR SCHEDULE AND PAINTING SPECS
 - REMOVE PTAC GRILLS - PREP AND PAINT AND REINSTALL
 - IF EXISTING - REMOVE SCREENS - LABEL, CLEAN AND REINSTALL AFTER EXTERIOR REFINISHING IS COMPLETE. CONFIRM QUANTITY OF NEW REQUIRED SCREENS WITH PM
 - PRIME AND PAINT PREFINISHED METAL ROOFING.
 - PAINT SERVICE YARD AND POOL ENCLOSURES INCLUDING GATES AND SOUND WALLS. PROTECT BRICK AND STONE PIERS.

SIGNAGE / LIGHTING

- INSTALL NEW BOX SIGN ON EXISTING FOUNDATION - VERIFY FOUNDATION SIZE PRIOR TO ORDERING.
- REMOVE AND REPLACE EXISTING CHANNEL LETTER SIGN. MAKE ANY BOX ADJUSTMENTS AS REQUIRED TO ACCOMMODATE NEW LETTER SPACING. PATCH PREVIOUS MOUNTING / HARDWARE LOCATIONS.

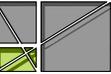


COLORS

	PT-01 SHERWIN WILLIAMS SW 7002 - DOWNY		PT-04 (ACCENT PAINT) SHERWIN WILLIAMS SW6356 - COPPER MOUNTAIN
	PT-02 SHERWIN WILLIAMS SW 7029 - AGREEABLE GRAY		PT-03 (METAL ROOFING) PPG CORAFLOX HAMPTONS GRAY TEX PONT29329P
	PT-03 SHERWIN WILLIAMS SW 7068 - GRIZZLE GRAY		

SHEET NOTES

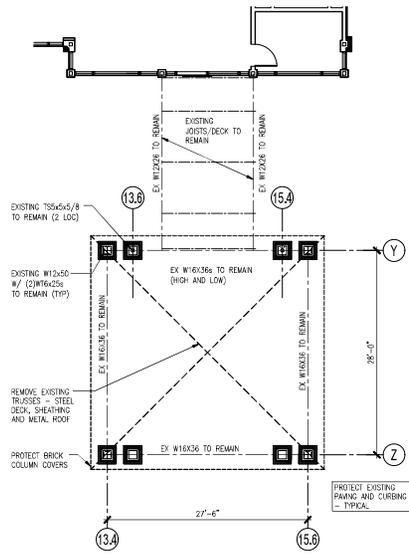
- NEW PAINTING TO INCLUDE ALL BUILDING AREAS INCLUDING THOSE HIDDEN OR NOT OTHERWISE SHOWN IN THE SET.
- DOWNPOUTS AND GRILLS TO BE PAINTED ARE NOT SHOWN TYPICALLY, BUT CONTRACTOR IS RESPONSIBLE FOR QUANTIFYING AND PAINTING THESE ITEMS.
- PT-03 GRILLS SHALL BE REMOVED AND SHOP PAINTED TO ADJACENT MATCH BODY COLOR (SEMI-GLOSS) THEN REINSTALLED.
- ADJURE OWNER OF ANY AREAS OF DISCREPANCY PRIOR TO PAINTING. VERIFY QUANTITIES OF REPAIR WORK WITH PROJECT MANAGER PRIOR TO ACCEPTANCE OF BBS.
- SERVICE YARDS, GATES AND POOL ENCLOSURE SHALL ALL BE PAINTED MAIN FIELD COLOR (PT-03) UNLESS NOTED OTHERWISE.
- GATES, METAL FENCING AND RAILINGS SHALL ALL BE PAINTED FIELD COLOR (PT-03) GLOSS SHEEN UNLESS NOTED OTHERWISE.



COURTYARD BY MARRIOTT
PAINTING DIAGRAMS
2416 - MARSHA S.U.C.C.

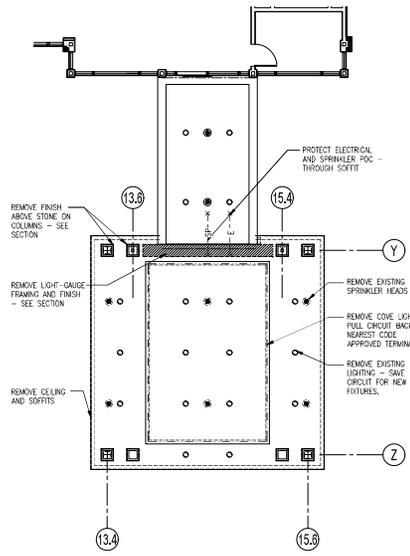
PAINTING DIAGRAMS
ISSUE DATE: 5.19.2025 | PERMIT

A91



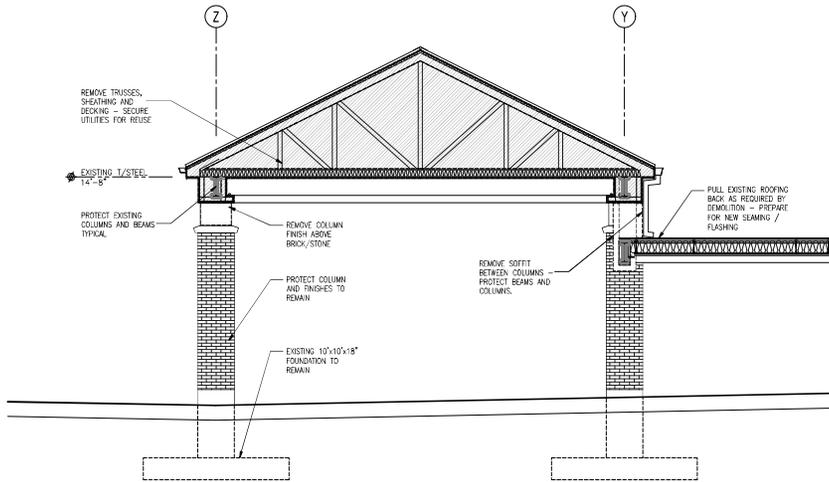
DEMOLITION PLAN

1/8" = 1'-0"



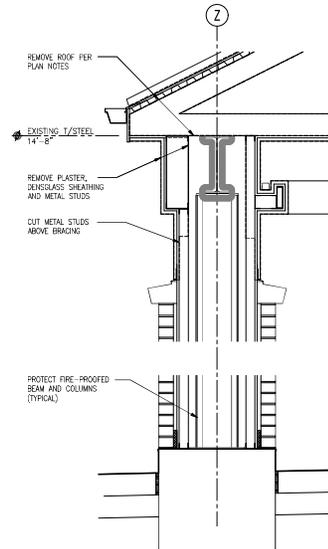
DEMO CEILING PLAN

1/8" = 1'-0"



DEMOLITION SECTION

1/4" = 1'-0"



ENLARGED TYPICAL COLUMN

3/4" = 1'-0"

SHEET NOTES

1. PROTECT EXISTING FINISHES TO REMAIN. REPAIR ANY CONSTRUCTION RELATED DAMAGE.
2. WHERE CIRCUITS ARE BEING ADJUSTED OR REMOVED, CONTRACTOR SHALL REVIEW AND OBTAIN RECEIPTS FOR ITEMS TO REMAIN ARE NOT OBTAINED, RUN NEW HOME-RUN CIRCUITS IF NECESSARY.

GREEN BUILDING - SEE SHEET A02

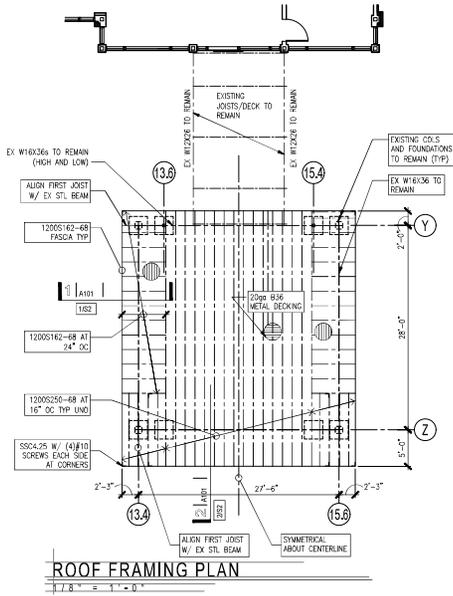
01. IN ACCORDANCE WITH THE MANDATORY MEASURES, CONTRACTOR MUST PROVIDE A WASTE MANAGEMENT PLAN TO RECYCLE AND/OR DIVERT WASTE FROM LAND FILL.



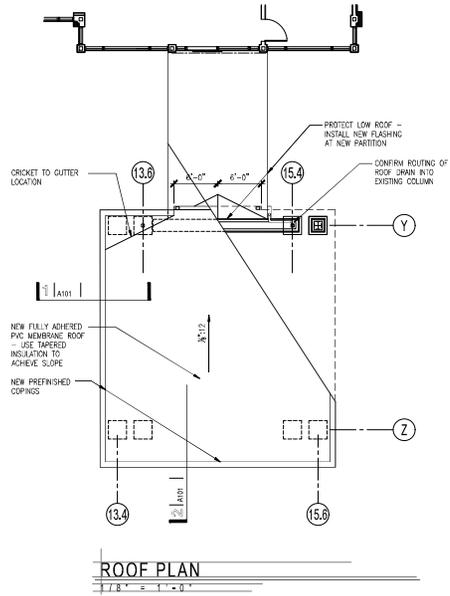
COURTYARD BY MARRIOTT
 CAMPBELL CALIFORNIA | HUNTINGTON HOTEL GROUP
 2416 - MARSHA S.L.C.C.

PORTE COCHERE
 DEMOLITION PLANS AND SECTIONS
 ISSUE DATE: 5.19.2025 | PERMIT

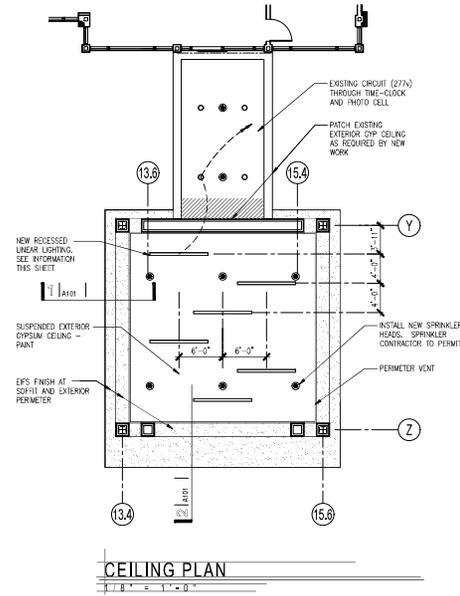
A101



ROOF FRAMING PLAN
1/8" = 1'-0"



ROOF PLAN
1/8" = 1'-0"



CEILING PLAN
1/8" = 1'-0"

ROOF DRAINAGE

1. RAINFALL RATE = 1.5 INCHES PER HOUR (PER TABLE D101.1 - SAN FRAN)
2. PER TABLE 1103.2 OF 2022 CALIFORNIA PLUMBING CODE
 - 2.1 1/4" PER FOOT SLOPE
 - 2.2 4" DIA = 3,760 S.F. ROOF AREA
 3. ACTUAL CONTRIBUTING PER DRAIN = 1,120 S.F.

RAFTER VENTING

1. AREA OF THE ATTIC SPACE = 1120 S.F.
2. VENT AREA REQUIRED 1/150 PER PARAGRAPH 1203.2.1
 - 2.1 1120 / 150 = 7.47 S.F. VENT REQUIRED
 3. 2" CONTINUOUS VENT AREA = 17 S.F. / 50% OPEN 8.5 S.F. TOTAL



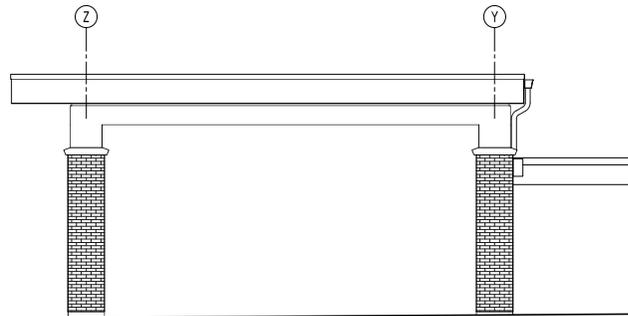
COURTYARD BY MARRIOTT
CAMPBELL CALIFORNIA | HUNTINGTON HOTEL GROUP
2416 - MARSHA S.U.C.C

PORTE COCHERE
DEMOLITION PLANS AND SECTIONS
ISSUE DATE: 5.19.2025 | PERMIT

A102



FRONT ELEVATION
1/4" = 1'-0"



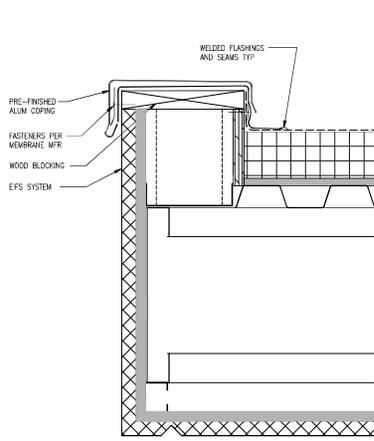
SIDE ELEVATION
1/4" = 1'-0"



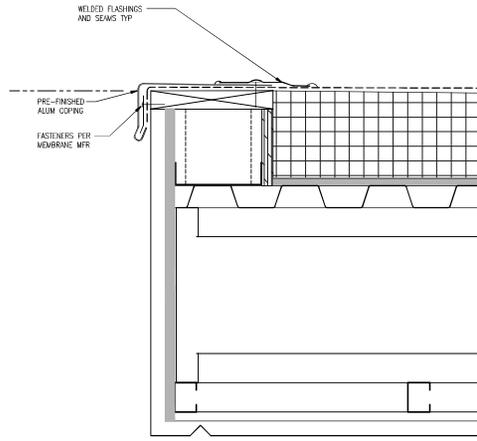
COURTYARD BY MARRIOTT
 CAMPBELL CALIFORNIA | HUNTINGTON HOTEL GROUP
 2416 - MARSHA S.L.C.C.

PORTE COCHERE
 SECTIONS AND DETAILS
 ISSUE DATE: 5.19.2025 | PERMIT

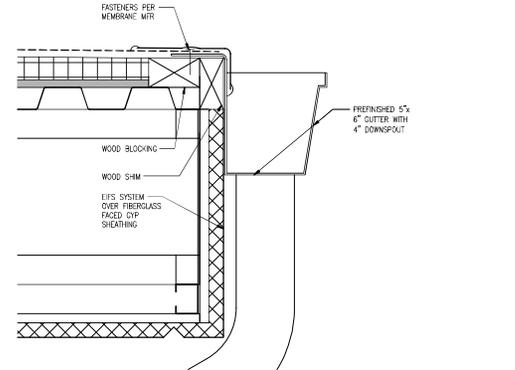
A103



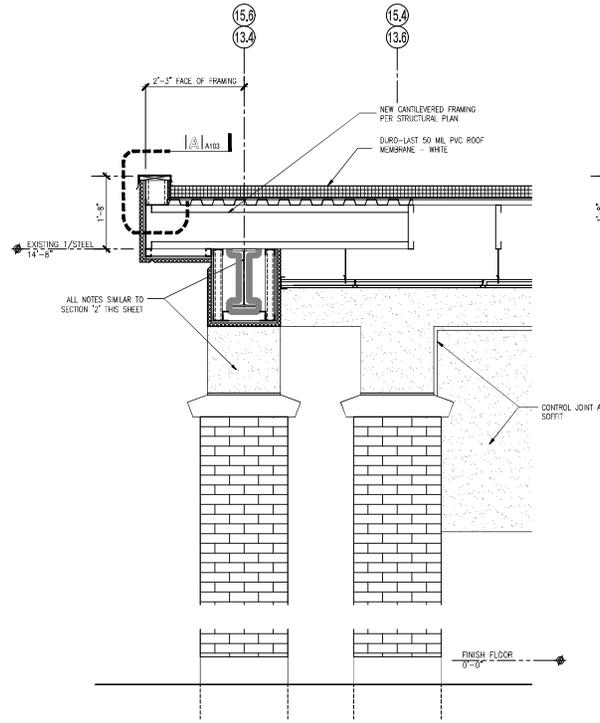
A DETAIL
 0'-4" = 1'-0"



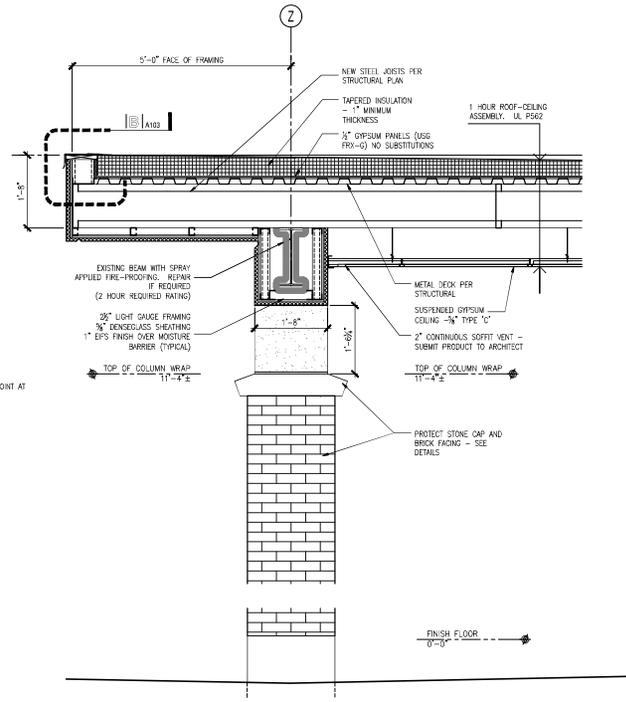
B DETAIL
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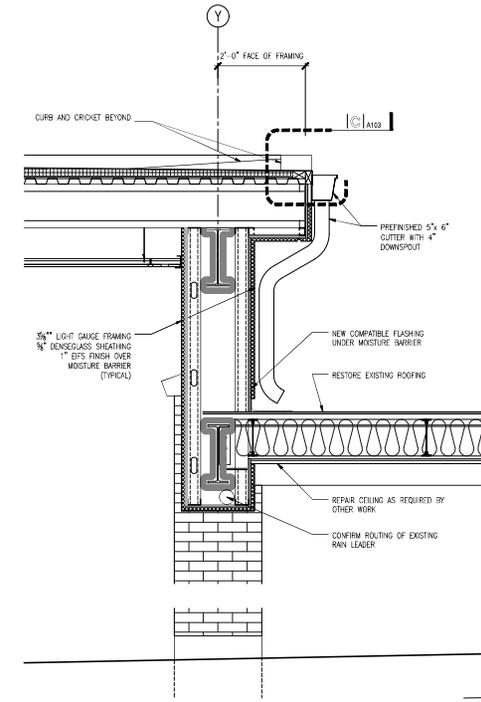
C DETAIL
 0'-4" = 1'-0"



1 SECTION
 0'-4" = 1'-0"



2 LONGITUDINAL SECTION
 0'-4" = 1'-0"



3 AT VESIBULE
 0'-4" = 1'-0"

DESIGN CRITERIA

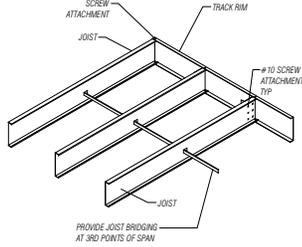
CODE:	2022 CALIFORNIA BUILDING CODE	
ROOF:		20 PSF
WIND:		
BASIC WIND SPEED:		95 MPH
RISK CATEGORY:	II	
EXPOSURE:	B	
TOPOGRAPHICAL FACTOR, K _z :		1.0
SEISMIC:		
SPECTRAL RESPONSE ACCELERATION, S ₁ :		1.627
SPECTRAL RESPONSE ACCELERATION, S _T :		0.6
SPECTRAL RESPONSE ACCELERATION, S _{0.5} :		1.302
SPECTRAL RESPONSE ACCELERATION, S _{0.1} :		0.88
SOIL SITE CLASS, F _a :		1.2
SOIL SITE CLASS, F _v :		1.7
SEISMIC DESIGN CATEGORY:		D

GENERAL CONDITIONS

- THE CONTRACTOR SHALL VERIFY AND REVIEW ALL ITEMS WITHIN THE DRAWINGS PRIOR TO PROCEEDING WITH THE WORK. NOTIFY THE ENGINEER/ARCHITECT IMMEDIATELY WITH ANY DISCREPANCIES.
- IF A SPECIFIC DETAIL IS NOT SHOWN FOR ANY PART OF THE WORK, THE CONSTRUCTION SHALL BE THE SAME AS FOR SIMILAR WORK.
- DIMENSIONS ARE NOT TO BE SCALED FROM THE PLANS, SECTIONS, OR DETAILS WITHIN THE DRAWINGS.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCE, AND PROCEDURES.
- ALL WORK SHALL CONFORM TO THE MINIMUM STANDARDS OF THE REFERENCED BUILDING AND ALL OTHER REGULATING AGENCIES EXERCISING AUTHORITY OVER ANY PORTION OF THE WORK.
- SPECIFIC NOTES AND DETAILS IN THE DRAWINGS SHALL TAKE PRECEDENCE OVER GENERAL NOTES AND SPECIFICATIONS.
- NOTIFY THE ENGINEER OF ALL CHANGES MADE IN THE FIELD PRIOR TO INSTALLATION.

COLD FORMED METAL (STEEL STUD)

- REFERENCE STANDARDS: AISI "S" SPECIFICATIONS FOR THE DESIGN OF COLD-FORMED STEEL STRUCTURAL MEMBERS.
- MEMBER AND ACCESSORY SIZES AND SECTION PROPERTIES PER PLAN.
- ALL MEMBERS AND ACCESSORIES ARE TO HAVE A YIELD STRESS OF 33 KSI (MIN) AND 50 KSI FOR 54 GAUGE AND THICKER UNLESS NOTED OTHERWISE IN THE DRAWINGS.



STEEL DECK

- REFERENCE STANDARDS: AISI "S" SPECIFICATIONS FOR THE DESIGN OF COLD-FORMED STEEL STRUCTURAL MEMBERS AND AWS D1.3-89 "SPECIFICATIONS FOR WELDING SHEET STEEL IN STRUCTURES".
- STEEL DECKING TYPE AND GAUGE PROPERTIES PER PLAN. USE VERCO DECKING OR APPROVED ALTERNATE.
- PROVIDE A MINIMUM 2" BEARING OVER SUPPORTING MEMBERS AT END AND 4" MINIMUM BEARING AT INTERIOR MEMBERS. PLACE UNITS END TO END PRIOR TO PERMANENTLY FASTENING. ALIGN RIBS OVER ENTIRE LENGTH.
- DECK FASTENING:
 - SUPPORTS PERPENDICULAR TO RIBS. SECURE TO SUPPORTING MEMBERS WITH #12 SCREW AT EVERY VALLEY, EIGHT (8) SCREWS PER 36" SHEET, L.I.N.O.
 - PARALLEL PANEL TO PANEL (SIDE LAP) CONNECTION. BUTION PUNCH AT THREE (3) PER 36" SHEET OR #12 SCREW AT 24" O.C.

SHOP DRAWINGS AND SUBMITTALS

- SUBMIT LAYOUT DRAWINGS IN PDF FORMAT FOR REVIEW OF:
 - COLD-FORMED STEEL

JOB SITE SAFETY

THE ENGINEER HAS NOT BEEN RETAINED OR COMPENSATED TO PROVIDE DESIGN AND/OR CONSTRUCTION REVIEW SERVICES RELATED TO THE CONTRACTOR'S SAFETY PRECAUTIONS OR TO MEANS, METHODS, TECHNIQUES OR PROCEDURES FOR THE CONTRACTOR TO PERFORM THE WORK. THE UNDERTAKING OF PERIODIC SITE VISITS BY THE ENGINEER SHALL NOT BE CONSTRUED AS SUPERVISION OF ACTUAL CONSTRUCTION NOR MAKE HIM RESPONSIBLE FOR PROVIDING A SAFE PLACE FOR THE PERFORMANCE OF THE WORK BY THE CONTRACTOR, SUB-CONTRACTOR OR ANY PERSON ON THE SITE.

ABBREVIATIONS:

A	BOLT	ANCHOR BOLT
ADDL	ADDITIONAL	
A.F.F.	ADJACENT FINISH FLOOR	
ALT	ALTERNATE	
APPROX	APPROXIMATE	
ARCH	ARCHITECTURAL	
BLOCKING	BLOCKING	
BM	BEAM	
B.O.O.	BOTTOM OF OPENING	
BTM	BOTTOM	
BRG	BEARING	
BTW	BETWEEN	
CLR	CLEAR	
CMU	CONCRETE MASONRY UNIT	
COL	COLUMN	
CONC	CONCRETE	
COND	CONDITION	
CONV	CONNECTION	
CONST	CONSTRUCTION	
CONT	CONTINUOUS	
DOB	DOUBLE	
DM	DIAMETER	
DIM	DIMENSION	
DL	DEAD LOAD	
EA	EACH	
EF	EACH FACE	
ELEV	ELEVATION	
EQ	EQUAL	
EQ	EQUAL	
EQUIP	EQUIPMENT	
ES	EACH SIDE	
EXIST	EXISTING	
EXT	EXTERIOR	
FD	FLOOR DRAIN	
FEN	FOUNDATION	
FF	FINISH FLOOR	
FG	FINISH GRADE	
FLR	FLOOR	
FLR	FLOOR FRAMED	
FT	FEET	
FTG	FOOTING	
FRT	FIRE RETARDANT TREATED	
FS	FAR SIDE	
GA	GAUGE	
GALV	GALVANIZED	
GLB	GLUE LAMINATED BEAM	
HDR	HEADER	
HGR	HANGER	
HORIZ	HORIZONTAL	
HT	HEIGHT	
I.F.	INSIDE FACE	
IN	INCH	
LL	LINE LOAD	
MAX	MAXIMUM	
MECH	MECHANICAL	
MFR	MANUFACTURER	
MIN	MINIMUM	
MISC	MISCELLANEOUS	
NIS	NEAR SIDE	
NTS	NOT TO SCALE	
O.C.	ON CENTER	
PARAL	PARALLEL	
PERP	PERPENDICULAR	
PSF	POUNDS PER SQUARE FOOT	
PSI	POUNDS PER SQUARE INCH	
PT	PRESSURE TREATED	
RAP	RAMMED AGGREGATE PER	
REIN	REINFORCING	
REQD	REQUIRED	
SCHD	SCHEDULE	
SEC	SECTION	
SF	SQUARE FEET	
SIM	SIMILAR	
SPEC	SPECIFICATIONS	
STD	STANDARD	
STL	STEEL	
STRUCT	STRUCTURAL	
SW	SHEARWALL	
THRU	THROUGH	
TOC	TOP OF CONCRETE	
TOF	TOP OF FOOTING	
T.O.O.	TOP OF OPENING	
TOS	TOP OF STEEL	
TOW	TOP OF WALL	
TS	TUBE STEEL	
TYP	TYPICAL	
U.N.O.	UNLESS NOTED OTHERWISE	
VERT	VERTICAL	
WI	WIRE	
WIF	WIDE FLANGE	
WHS	WELDED HEADED STUD	
WTS	WELDED THREADED STUD	
WT	WEIGHT	
WWR	WELDED WIRE REINFORCING	

SHEET INDEX

- S1- GENERAL NOTES
- S2- FRAMING DETAILS

PROJECT MANAGER: MS DESIGNED BY: MS, SF DRAWN BY: SF CHECKED BY: MS

NO.	DATE	BY	REVISION

FACET

P: 281.202.2624
 www.facet.com
 1000 14th Ave NE
 Federal Way, WA 98003
 Phone: 206.251.1515
 Fax: 206.251.1515

PROFESSIONAL SEAL
 PORTER COCHERE ARCHITECTURAL
 5114
 Exp. 6/26
 STATE OF CALIFORNIA

5/13/2025
PORTE COCHERE
 COURTYARD BY IMBROTTI
 CAPRELLI, CA
 PROJECT NO: 2504.0468.00

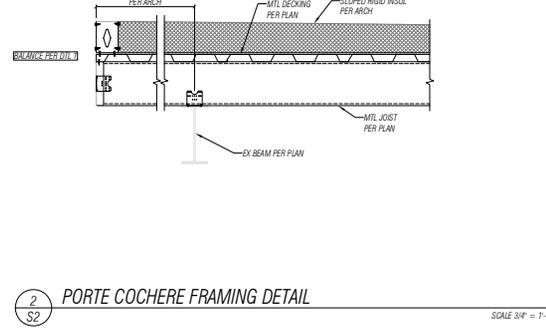
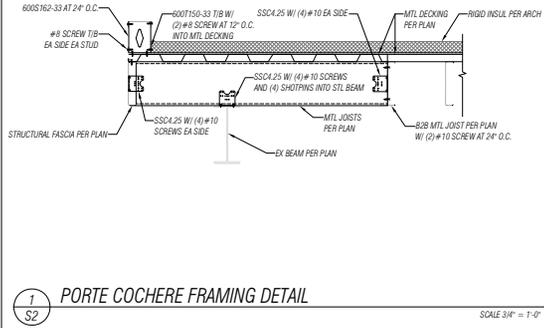
GENERAL NOTES

DATE: MAY 8, 2025

PLAN NUMBER:

S1

PRINCIPAL IN CHARGE PROJECT MANAGER IN CHARGE DESIGNER BY ME OF DRAWN BY: SF CHECKED BY: MS



1 S2 PORTE COCHERE FRAMING DETAIL SCALE 3/4" = 1'-0"

2 S2 PORTE COCHERE FRAMING DETAIL SCALE 3/4" = 1'-0"

NO.	DATE	BY	REVISION

FACET
 P: 260 COLLEGE
 www.facetinc.com
 1000 JEFFERSON
 SUITE 100
 BOSTON, MA 02115
 PHONE: 617.261.1000 FACILITY: 617.261.1000

PROFESSIONAL SEAL
 MICHAEL C. SCHMIDT
 5114
 Exp. 6/26
 STATE OF CALIFORNIA
 5/13/2025

PORTE COCHERE
 COURTYARD BY MARKET
 CAPRELL, CA
 PROJECT NO: 2504.0466.00

FRAMING DETAILS
 DATE: MAY 9, 2025
 PLAN NUMBER:
S2



Courtyard by Marriott
Campbell CA

Exterior Design Review

September 23, 2025
Mattoni Design Studio

COURTYARD
Marriott

Courtyard Brand

- Gen 2 – 1991
- Rooms above lobby for tighter sites
- Green metal roof emerges as defining feature



Courtyard Brand

- Gen 3 – 1997
- Central tower feature transitions to curve
- Green metal roof transitions to barrel vault



Courtyard Brand

- Gen 4 – 2005
- More urban
- Green metal translated into vertical feature
- Porte cochere becomes flat



Courtyard Brand

- Current design
- Clean aesthetic
- Porte cochere becomes minimalistic
- Urban / downtown locations



Courtyard Brand

- First generations were 21 years old.
- Marriott required exterior renovations to modernize facades to uphold the brand vision
- 2018 guidelines



Exterior Facade Gen 1 Overview



1 - Site & Building Exterior

General / Site Entrance / Porte Cochere

3. Reimage Porte Cochere. **Remove the pitched roof** and flatten to be consistent with the building architecture.
4. Repair and paint ceiling at porte cochere.

Architectural Façade / Envelope

5. **Repaint building exterior to current standards.**
6. Repair exterior building finishes to a "like-new" condition.
7. Verify the condition of all exterior windows, doors, frames, sills, and seals, and repair/replace as necessary to prevent wind, rain and noise from leaking into building.
8. Repair all service doors at building exterior.

Landscape / Visual Barriers

9. Remove and replace all dead or misshapen landscaping.

Miscellaneous Site Features

Lighting and landscaping

Courtyard Brand

- All generations – when required to relicense are required to perform exterior updates
- Cooler color palettes
- Simplified porte cochere – most important new element
- 2025 guidelines



Examples

- Cypress California
- Eugene Oregon



Existing

- Old signage
- Warm colors
- Metal roof



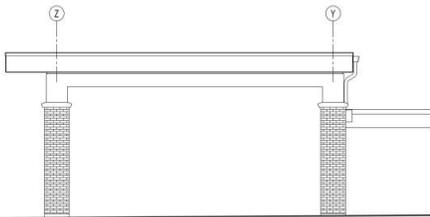
Initial Proposal

- Painted only
- New signage
- Marriott rejected



Proposed

- Paint roof only
- Hipped roof replaced with flat truss



SIDE ELEVATION



Context

- New colors blend well with modern aesthetic of the surrounding buildings
- Minimal profile and horizontal lines of porte cochere are also more appropriate.





Mattoni Design Studio

Joseph M. Mattoni, AIA
President

Joem@MattoniDesignStudio.com





CITY OF CAMPBELL · PLANNING COMMISSION
Staff Report · October 28, 2025

PLN-2025-111

Shirvani, H.

Public Hearing to consider the request of Hamid Shirvani to legalize dancing and live entertainment, limited to karaoke, live bands, and disc jockey (DJ) performances, conducted in association with an existing restaurant and bar (dba Effie’s) with nonconforming onsite alcohol sales, and to allow ongoing "late night" business hours (12:30 AM closing on Wednesdays and Thursdays; 1:30 AM closing on Fridays and Saturdays), on property located at **331 W. Hacienda Avenue**. The application under consideration is a Conditional Use Permit. File No.: PLN-2025-111

STAFF RECOMMENDATION

That the Planning Commission take the following action:

- 1. **Adopt a Resolution** (reference **Attachment A**), approving a Conditional Use Permit.

ENVIRONMENTAL (CEQA) DETERMINATION

Staff recommends that the Planning Commission accept the determination that this project is Categorically Exempt under Section 15301 of the [California Environmental Quality Act \(CEQA\) Guidelines](#), pertaining to the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of an existing private structure.

PROJECT DATA

Zoning Designation:	GC (General Commercial)	
General Plan Designation:	<i>General Commercial</i>	
Net Lot Area:	33,367 square feet	
Building Square Footage:	5,950 square feet	
Parking:	55 stalls	0 stalls required (AB 2097 area)
Hours of Operation:		
Business Hours:	7:00 AM – 11:00 PM, Sunday through Tuesday 7:00 AM – 12:30 AM, Wednesday and Thursday 7:00 AM – 1:30 AM, Friday and Saturday	
Operational Hours:	6:00 AM – 11:30 PM, Sunday through Tuesday 6:00 AM – 1:00 AM, Wednesday and Thursday 6:00 AM – 2:00 AM, Friday and Saturday	
Live Entertainment:	Until 11:00 PM, Sunday through Tuesday Until 12:00 AM, Wednesday through Saturday	

DISCUSSION

Project Site: The project site is an approximately 33,000 square-foot parcel located along West Hacienda Avenue, one parcel west of South Winchester Boulevard. The property borders single-family residences to the north, an Exxon service station to the east, and the Santa Clara Valley Water District percolation ponds to the west, as shown on the aerial map, below. The property is within the GC (General Commercial) Zoning District and the corresponding *General Commercial* General Plan land use district.



Background: On September 18, 1967, the Planning Commission granted Site Approval "["S" 67-57](#)" for construction of a new restaurant/bar building on the subject property. A restaurant and bar has operated continuously since that time, including Effie's since 1988. Due to the age of the original approval, its alcoholic beverage service predates the City's requirement for a Conditional Use Permit and is therefore considered a legal nonconforming activity.

The City initiated a code enforcement case (IWQ2019-2666) in early 2019 for live entertainment activity occurring without a Conditional Use Permit after Police Department staff became aware of karaoke being advertised on the business's website. Following issuance of the violation notice, the then-business owner contended that the karaoke activity had been ongoing since 1988 and should therefore be considered a nonconforming use and "grandfathered." However, a review of the 1985 Zoning Code confirmed that live entertainment had already been designated a conditional use at that time, and the City determined that no nonconforming status had been vested. The City issued a similar warning notice for unpermitted live entertainment to the owner in 1994.

As part of the City's recent efforts to resolve the backlog of code enforcement cases that accumulated during the COVID-19 pandemic, a new violation notice was issued for the 2019 case. This notice was mailed earlier this year after the business had changed ownership and was purchased by the applicant, Mr. Hamid Shirvani, in July 2024. As a result, the current application seeks to resolve this longstanding code issue by securing the necessary approval.

Proposed Project: The submitted application for a Conditional Use Permit would allow ancillary live entertainment within the existing restaurant and bar, inclusive of karaoke, live band, and disc jockey (DJ) performances (reference **Attachment B** – Project Description). As depicted on Project Plans (reference **Attachment C**), live entertainment would be conducted from the designated "entertainment area" and would conclude at 11:00 PM on Sundays through Tuesdays and at 12:00 AM on Wednesdays through Saturdays.

The Conditional Use Permit application would also authorize the "late-night business" hours of operation noted, below, as well as formally recognize the existing legal nonconforming alcoholic beverage service associated with the establishment. For reference, "business hours" are the hours when the establishment is open to the public, while "operational hours" include additional time before opening and after closing for staff preparation and clean-up activities.

Business Hours: 7:00 AM – 11:00 PM, Sunday through Tuesday
7:00 AM – 12:30 AM, Wednesday and Thursday
7:00 AM – 1:30 AM, Friday and Saturday

Operational Hours: 6:00 AM – 11:30 PM, Sunday through Tuesday
6:00 AM – 1:00 AM, Wednesday and Thursday
6:00 AM – 2:00 AM, Friday and Saturday

ANALYSIS

Findings for Approval: To grant a land use permit, the decision-making body must determine that the project satisfies all codified findings for approval. These findings form the evidentiary basis for approving or denying a land use request and for imposing conditions necessary to support the findings. The applicable findings vary by the type of approval under consideration (e.g., Site and Architectural Review, Tree Removal Permit). The following analysis presents each finding in *italics* for a Conditional Use Permit pursuant to CMC Sec. 21.46.040, followed by an explanation of how the proposed project satisfies it.¹

A. *The proposed use is allowed within the applicable zoning district with Conditional Use Permit approval, and complies with all other applicable provisions of this Zoning Code and the Campbell Municipal Code;*

Yes. The project includes two distinct land uses, "dancing and/or live entertainment establishment" and "late-night activities," each permitted in the GC (General Commercial) Zoning District subject to Conditional Use Permit approval. These uses reflect long-standing aspects of the business's operation and are being formally recognized through this application to ensure consistency with current zoning standards and the City's review procedures. Each use is defined and discussed on the following page.

- *Dancing and/or Live Entertainment Establishment*: The proposal includes a request to legalize karaoke and DJ performances conducted within the bar area using standard audio equipment. The business owner also requests allowance for live bands to continue Effie's

¹ This application does not require the onsite-alcohol findings provided in CMC Section 21.36.144 (Onsite alcohol sales) to be made by the Planning Commission because the business's onsite alcohol service is an existing legal nonconforming use pursuant to CMC Section 21.58.040.G (Nonconforming Due to Lack of Conditional Use Permit), such that this application does propose the "establishment" of a new onsite alcohol sales facility.

"tradition of hosting summer concerts that feature local bands once a week during the summer months, as well as for special occasions such as New Year's Eve." Although the proposal does not identify a designated dance floor or platform, patrons may dance on an incidental basis.

"Dancing and/or live entertainment establishment" means a commercial facility that offers a venue intended to allow patrons to dance and/or listen to live entertainment, as defined by Section 5.24.010(b). Does not include non-commercial expressive activity protected by the United States or California constitutions or the listening of recorded music without a dancing venue.

- *Late Night Activities*: The Zoning Code defines "late night activities" as any business-related activity occurring after 11:00 PM or before 6:00 AM. This application proposes public closing times of 12:30 AM, Wednesday and Thursday, and 1:30 AM, Friday and Saturday. Although the applicant notes that the restaurant has historically operated during these hours, such operations are not considered legal nonconforming uses pursuant to CMC Section 21.58.040.B.3. In 2001, the City Council adopted an ordinance that amortized all businesses with "late night" hours, requiring them to either conform to current regulations or obtain a Conditional Use Permit within two years. This action effectively eliminated any prior nonconforming status for late-night operations, ensuring that such activities are now subject to discretionary review to address potential impacts related to noise, security, and neighborhood compatibility.

"Late night activities" means land use activities operating between the hours of 11:00 p.m. and 6:00 a.m., including, but not limited to, the provision of goods and services to the public and all ancillary activities such as property maintenance, janitorial services, street and parking lot sweeping, deliveries, and similar activities. "Late night activities" do not include the lawful, reasonable and customary use of residential uses or professional offices in a manner that does not interfere with the reasonable use and enjoyment of other properties.

B. The proposed use is consistent with the General Plan;

Yes. The General Plan land use designation for the project site is General Commercial (GC). As described in the General Plan, this designation is intended to accommodate "commercial uses that need exposure to high volumes of automobile traffic or access to transit corridors," including restaurants, entertainment venues, and other customer-serving businesses. The site is located along West Hacienda Avenue, one parcel west of Winchester Boulevard, an arterial that serves as a key commercial corridor providing regional access and visibility.

The proposed Conditional Use Permit would maintain a long-established restaurant and bar use in a location well-suited for this type of commercial activity. The project supports the General Plan's Economic Development and Land Use Elements goals by promoting business retention (Action ED-2.5), providing neighborhood-serving dining within walking distance of nearby residences (Policy LU-1.14), and maintaining a variety of convenient and attractive commercial and entertainment uses (Policy LU-4.7). Through this process, the City can ensure the business is well kept and operated to limit impacts to adjacent uses (Action LU-2.b) and that late-night activities are reviewed for compatibility with nearby residential areas (Policy N-1.7).

Policy LU-1.14: Encourage neighborhood services within walking distance of residential uses.

Policy LU-4.7: Maintain a variety of attractive and convenient commercial and office uses that provide needed goods, services, and entertainment.

- Action LU-2.b:** Through the development review and permit process, ensure that conditions of approval are adopted that require businesses to be well kept and operated in a way that limits impacts to adjacent uses.
- Action ED-2.5:** Encourage retention and expansion of local businesses as appropriate to provide a range of jobs and services to residents, to retain revenue in the city, and respond to community needs for key amenities and services.
- Policy N-1.7:** Continue to recognize that "late night activities" (land use activities operating from 11:00 p.m. to 6:00 a.m., not including the lawful, reasonable, and customary use of residential uses or professional offices that does not interfere with the reasonable use and enjoyment of other properties) may present a potential noise impact, and should be carefully reviewed to ensure compatibility with adjacent residences.

C. The proposed site is adequate in terms of size and shape to accommodate the fences and walls, landscaping, parking and loading facilities, yards, and other development features required in order to integrate the use with uses in the surrounding area;

Yes. The project site is fully developed with an existing restaurant and bar use that has operated for several decades. The site includes a surface parking lot with adequate capacity to serve the business, as well as established perimeter landscaping that provides visual buffering from adjacent residential properties. The existing site layout accommodates building access, deliveries, and refuse collection. The property's size and configuration are appropriate for the continued operation of the use, and no expansion of the building footprint or paved area is proposed that would diminish site functionality or compatibility with neighboring commercial and residential uses.

D. The proposed site is adequately served by streets of sufficient capacity to carry the kind and quantity of traffic the use would be expected to generate;

Yes. The project involves the continued operation of a long-established restaurant and bar use and will not introduce new activities that would substantially increase vehicle trips or traffic volumes in the surrounding area. The site is accessed from West Hacienda Avenue, a "residential collector" street with direct connectivity to Winchester Boulevard, a "class II arterial" with sufficient capacity to accommodate the traffic generated by the business. The proposed Conditional Use Permit does not expand the building footprint or seating capacity, and therefore no traffic study was required.

E. The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses on-site and in the vicinity of the subject property; and

Yes. The restaurant and bar are located along West Hacienda Avenue, immediately behind single-family residences to the north, within a corridor that transitions between commercial and residential uses. The proposed Conditional Use Permit would formalize ancillary live entertainment, which has been an ongoing—albeit unpermitted—activity **that has not generated any citizen complaints** (as noted, the 2019 code case was staff-initiated). The existing building and site configuration adequately accommodate the use without creating land use conflicts or adverse impacts on surrounding properties. The adjoining Exxon service station to the east is a commercially designated property that does not present any existing or future land use conflicts.

F. The establishment, maintenance, or operation of the proposed use at the location proposed will not be detrimental to the comfort, health, morals, peace, safety, or general welfare of persons residing or working in the neighborhood of the proposed use, or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city.

Yes. The Police Department reviewed the proposal and raised no objections. There have been no recent complaints, and the recommended conditions of approval establish clear performance standards for noise, hours of operation, and entertainment activities, as well as enforcement mechanisms that allow the City to modify or revoke the permit if issues arise. As such, the continued operation of the restaurant and bar, including ancillary live entertainment, will not adversely affect surrounding properties or the general welfare of the community. These measures ensure that the establishment operates responsibly and remains compatible with adjacent residential and commercial uses, preserving the comfort and safety of nearby residents and the overall integrity of the neighborhood.

Public Comment: No public comment has been received.

Attachments:

- A. Draft Resolution
- B. Project Description
- C. Project Plans

Prepared by:



Daniel Fama, Senior Planner

Approved by:



Rob Eastwood, Community Development Director

RESOLUTION NO. 2025-xxx

BEING A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CAMPBELL GRANTING APPROVAL OF A CONDITIONAL USE PERMIT TO LEGALIZE DANCING AND LIVE ENTERTAINMENT, LIMITED TO KARAOKE, LIVE BANDS, AND DISC JOCKEY (DJ) PERFORMANCES, CONDUCTED IN ASSOCIATION WITH AN EXISTING RESTAURANT AND BAR (DBA EFFIE'S) WITH NONCONFORMING ONSITE ALCOHOL SALES, AND TO ALLOW ONGOING "LATE NIGHT" BUSINESS HOURS (12:30 AM CLOSING ON WEDNESDAYS AND THURSDAYS; 1:30 AM CLOSING ON FRIDAYS AND SATURDAYS), ON PROPERTY LOCATED AT **331 W. HACIENDA AVENUE**. FILE NO.: PLN-2025-111

After notification and public hearing, as specified by law and after presentation by the Community Development Director, proponents and opponents, the hearing was closed.

After due consideration of all evidence presented, the Planning Commission did find as follows with respect to the approval of a Conditional Use Permit (PLN-2025-111):

1. The project site is zoned GC (General Commercial) on the City of Campbell Zoning Map.
2. The project site is designated General Commercial on the City of Campbell General Plan Land Use Diagram.
3. The project site consists of an approximately 33,000-square-foot parcel developed with an existing restaurant and bar (Effie's) located on the north side of West Hacienda Avenue, west of Winchester Boulevard.
4. The property borders single-family residences to the north, an Exxon service station to the east, and the Santa Clara Valley Water District percolation ponds to the west.
5. On September 18, 1967, the Planning Commission approved Site Approval "S" 67-57 for construction of the existing restaurant and bar building. A restaurant and bar use has operated continuously at this location since that time, including Effie's since 1988.
6. The City initiated a code enforcement case (IWQ2019-2666) in early 2019 for live entertainment activity (karaoke) occurring without a Conditional Use Permit. Following issuance of a violation notice, the then-business owner asserted that the live entertainment had been ongoing since 1988 and therefore should be considered a nonconforming use. However, review of the 1985 Zoning Code confirmed that live entertainment was already designated as a conditional use at that time, and the City determined that no nonconforming status had been vested. A similar warning notice for unpermitted live entertainment had also been issued in 1994. In 2024, as part of the City's efforts to resolve a backlog of open cases, a new notice was issued for the outstanding 2019 case.

7. The proposed project is an application for a Conditional Use Permit to legalize ancillary live entertainment, limited to karaoke, live bands, and disc jockey (DJ) performances, and to authorize late-night business operations until 12:30 a.m. on Wednesdays and Thursdays and 1:30 a.m. on Fridays and Saturdays, without building expansion or exterior modification.
8. The proposed "dancing and/or Live Entertainment Establishment" and "late-night activity" uses are both allowable in the GC (General Commercial) Zoning District subject to approval of a Conditional Use Permit.
9. Because the original 1967 approval and associated alcoholic beverage service predate the City's requirement for a Conditional Use Permit for onsite alcohol sales, the use is recognized as a legal nonconforming activity consistent with CMC Section 21.58.040.G (Nonconforming Due to Lack of Conditional Use Permit) and is further protected under Business and Professions Code Section 23790, which allows the continued operation of existing, continuously operated, licensed premises under the same license classification and mode of operation, notwithstanding later-enacted zoning regulations.
10. Although the restaurant has historically operated after 11:00 p.m., such operations are not considered a legal nonconforming use pursuant to CMC Section 21.58.040.B.3. In 2001, the City Council adopted Ordinance No. 2002 that amortized all businesses with "late-night" hours, requiring them to either conform to current regulations or obtain a Conditional Use Permit within two years. This action eliminated any prior nonconforming status for late-night operations, ensuring that such activities are subject to discretionary review to address potential impacts related to noise, security, and neighborhood compatibility.
11. The proposed project is consistent with the General Commercial General Plan land use designation, which supports a range of commercial uses, including restaurants and entertainment establishments, that contribute to the city's economic vitality.
12. Applicable General Plan Policies considered by the Planning Commission included, but were not limited to, the following:

Policy LU-1.14: Encourage neighborhood services within walking distance of residential uses.

Policy LU-4.7: Maintain a variety of attractive and convenient commercial and office uses that provide needed goods, services, and entertainment.

Action LU-2.b: Through the development review and permit process, ensure that conditions of approval are adopted that require businesses to be well kept and operated in a way that limits impacts to adjacent uses.

Action ED-2.5: Encourage retention and expansion of local businesses as appropriate to provide a range of jobs and services to residents, to retain revenue in the city, and respond to community needs for key amenities and services.

Policy N-1.7: Continue to recognize that "late night activities" (land use activities operating from 11:00 p.m. to 6:00 a.m., not including the lawful, reasonable, and customary use of residential uses or professional offices that does not interfere with the reasonable use and enjoyment of other properties) may present a potential noise impact, and should be carefully reviewed to ensure compatibility with adjacent residences.

13. The recommended conditions of approval include specific operational standards for noise that require compliance with the City's noise regulations and prohibit sound from unreasonably interfering with the use and enjoyment of nearby properties. These requirements ensure that amplified music and live entertainment remain within acceptable limits and provide a clear enforcement mechanism allowing the City to reduce hours or modify the permit if verified noise complaints occur. Implementation of these standards will prevent the use from becoming a nuisance to adjacent residential or commercial properties.
14. The Police Department reviewed the proposal and raised no objections, finding that the business can continue to operate safely under the proposed conditions.
15. In review of the proposed project, the Planning Commission also weighed the public need for, and the benefit to be derived from, the project, against any impacts it may cause.
16. There is a reasonable relationship and a rough proportionality between the Conditions of Approval and the impacts of the project

Conditional Use Permit Findings (CMC Sec. 21.46.040):

1. The proposed use is allowed within the applicable zoning district with Conditional Use Permit approval, and complies with all other applicable provisions of this Zoning Code and the Campbell Municipal Code;
2. The proposed use is consistent with the General Plan;
3. The proposed site is adequate in terms of size and shape to accommodate the fences and walls, landscaping, parking and loading facilities, yards, and other development features required in order to integrate the use with uses in the surrounding area;
4. The proposed site is adequately served by streets of sufficient capacity to carry the kind and quantity of traffic the use would be expected to generate;
5. The design, location, size, and operating characteristics of the proposed use are compatible with the existing and future land uses on-site and in the vicinity of the subject property;
6. The establishment, maintenance, or operation of the proposed use at the location proposed will not be detrimental to the comfort, health, morals, peace, safety, or general welfare of persons residing or working in the neighborhood of the proposed use, or be detrimental or injurious to property and improvements in the neighborhood or to the general welfare of the city;

Environmental Finding(s) (CMC Sec. 21.38.050):

1. The project is Categorically Exempt under Section 15301 (Class 1) of the California Environmental Quality Act (CEQA), pertaining to the operation and leasing, and minor alteration of an existing private structure; and

2. No substantial evidence has been presented which shows that the project, as currently presented and subject to the required conditions of approval, will have a significant adverse impact on the environment.

THEREFORE, BE IT RESOLVED that the Planning Commission approves a Conditional Use Permit (PLN-2025-111) to legalize dancing and live entertainment, limited to karaoke, live bands, and disc jockey (DJ) performances, conducted in association with an existing restaurant and bar (dba Effie's) with nonconforming onsite alcohol sales, and to allow ongoing "late night" business hours (12:30 AM closing on Wednesdays and Thursdays; 1:30 AM closing on Fridays and Saturdays), on property located at **331 W. Hacienda Avenue**, subject to the attached Conditions of Approval (attached **Exhibit A**).

PASSED AND ADOPTED this 28th day of October 2025, by the following roll call vote:

AYES: Commissioners:

NOES: Commissioners:

ABSENT: Commissioners:

ABSTAIN: Commissioners:

APPROVED: _____
Matt Kamkar, Chair

ATTEST: _____
Rob Eastwood, Secretary

CONDITIONS OF APPROVAL
Conditional Use Permit Modification
(PLN-2025-111)

Where approval by the Director of Community Development, City Engineer, Public Works Director, City Attorney or Fire Department is required, that review shall be for compliance with all applicable conditions of approval, adopted policies and guidelines, ordinances, laws and regulations and accepted engineering practices for the item under review. Additionally, the applicant is hereby notified that he/she is required to comply with all applicable Codes or Ordinances of the City of Campbell and the State of California that pertain to this development and are not herein specified.

PLANNING DEPARTMENT:

1. Approved Project: Approval is granted for a Conditional Use Permit (PLN-2025-111) to legalize dancing and live entertainment, limited to karaoke, live bands, and disc jockey (DJ) performances, conducted in association with an existing restaurant and bar (dba Effie's) with nonconforming onsite alcohol sales, and to allow ongoing "late night" business hours (12:30 AM closing on Wednesdays and Thursdays; 1:30 AM closing on Fridays and Saturdays), on property located at **331 W. Hacienda Avenue**. The project shall substantially conform to the Approved Project Plans included as Attachment B to the Planning Commission staff report dated October 28, 2025, except as modified by the Conditions of Approval contained herein.
2. Permit Expiration: Approval of the Conditional Use Permit ("Approval") shall be valid for one (1) year from the effective date of the Planning Commission action. Within this one-year period, a Live Entertainment Permit in compliance with Chapter 5.24 (Live Entertainment) of the Campbell Municipal Code (CMC) must be obtained. Failure to meet this deadline shall result in the Approval being rendered void. Once established, this Approval shall be valid in perpetuity on the Project Site, subject to continued maintenance of an ABC liquor license, as provided for in Condition of Approval No. 3 (Operational Standards). Discontinuation of alcohol service for a continuous period of twelve months, as evidenced by surrender or revocation of the ABC license, shall void the Approval upon an affirmative determination by the Planning Commission in a public hearing that the use has been discontinued.
3. Operational Standards: The standard restaurant and bar use (hereafter referred to as the "establishment") operating pursuant to this Approval shall conform to the following operational standards. Significant deviations from these standards, as determined by the Community Development Director, shall require approval of a Modification to the Conditional Use Permit.
 - a. **Approved Use**: The approved use is a "restaurant, standard" with "late night activities" and ancillary full-service "onsite alcohol sales" and "dancing and live entertainment," as defined by CMC Section 21.72.020 in effect at the time of this approval and as limited by the operational standards listed herein. At no time shall the approved use be operated as a "quick service restaurant," as a stand-alone bar/tavern (a primary "onsite alcohol sales" use), or as a "nightclub" (an establishment principally intended to offer live entertainment and dancing).

- b. **Hours of Operations.** Hours of operation shall be as follows. Patrons shall vacate the establishment by the end of "Business Hours", and all employees shall depart by the end of "Operational Hours".
- Business Hours: 7:00 AM – 11:00 PM, Sunday through Tuesday
7:00 AM – 12:30 AM, Wednesday and Thursday
7:00 AM – 1:30 AM, Friday and Saturday
 - Operational Hours: 6:00 AM – 11:30 PM, Sunday through Tuesday
6:00 AM – 1:00 AM, Wednesday and Thursday
6:00 AM – 2:00 AM, Friday and Saturday
- c. **Live Entertainment:** Ancillary live entertainment is permitted within the establishment, subject to the following standards:
- i. Allowance: Live entertainment is limited to karaoke, live bands, and disc jockey (DJ) performances. Other forms of live entertainment are prohibited.
 - ii. Location: Live entertainment shall be conducted only from the designated "entertainment area", as depicted on the Approved Project Plans.
 - iii. Live Entertainment Hours: Live entertainment shall be permitted daily, ending at 11:00 PM on Sundays through Tuesdays, and at 12:00 AM, Wednesdays through Saturdays.
 - iv. Noise: Live entertainment shall comply with the noise standards provided in Condition of Approval No. 3.o (Noise). If the City receives verified noise complaints, the establishment owner shall cooperate with the City to address them through operational changes, which may include reducing amplification levels, relocating speakers, and/or preparing a noise study at the owner's expense. If such measures do not resolve the complaints, the Community Development Director may curtail the approved "live entertainment hours", consistent with Condition of Approval No. 6 (Revocation of Permit).
 - v. Dancing: Dancing is permitted only on an incidental basis. No portion of the dining area or other patron space shall be cleared, reconfigured, or designated to create a dance floor, and furnishings shall not be removed or rearranged to facilitate dancing. At all times, required aisles, egress paths, and seating configurations shown on the Approved Project Plans shall be maintained.
 - vi. Live Entertainment Permit: The establishment owner shall secure approval of a Live Entertainment Permit in compliance with CMC Chapter 5.24 prior to the commencement of any live entertainment performances.
 - vii. Security: On-site security shall be provided if required by a Live Entertainment Permit.
 - viii. Cover Charge: At no time shall a cover charge be required or a donation necessary in order to patronize the establishment.

- d. **Liquor License:** The owner of the establishment shall maintain in good standing a Type 47 (On-Sale General – Eating Place) or Type 41 (On-Sale Beer and Wine – Eating Place) license issued by the California Department of Alcoholic Beverage Control (ABC) for the sale of beer, wine, and/or distilled spirits for on-site consumption in conjunction with the operation of the establishment. The retail sale of beer and/or wine products for off-site consumption shall be prohibited.
- e. **Special Events:** The Community Development Director may allow special events through consideration of a Temporary Use Permit application in compliance with Chapter 21.45 of the Campbell Municipal Code, even if such events would otherwise conflict with the Conditions of Approval provided herein.
- f. **Business License:** The establishment shall be required to obtain and maintain a City business license at all times.
- g. **Food Service:** A full-service food menu, including entrées and side dishes customarily offered by the establishment, shall be available from opening until 10:00 PM. Thereafter, a limited menu (e.g., appetizers or small plates) shall remain available until the end of business hours.
- h. **Age Restriction:** At no time shall the establishment impose an age restriction for patronage. Minors shall be permitted within the establishment at all times that it is open to the public.
- i. **Floor Plan Layout:** Tables and chairs shall be maintained in a configuration consistent with the Approved Project Plans. The designated "bar" area shall not be expanded into the "dining area".
- j. **Employee Training:** The establishment shall use an employee training manual that addresses alcoholic beverage service consistent with the standards of the California Restaurant Association and the ABC.
- k. **Designated Driver Program:** The establishment shall maintain and actively promote a designated driver program (e.g., complimentary non-alcoholic beverages for designated drivers), including posting in a conspicuous place contact information for local designated driver services.
- l. **Maximum Occupancy:** Patron occupancy shall not exceed the occupant loads calculated in accordance with the California Building Code (CBC). The establishment owner is responsible for implementing entrance controls to ensure that occupancy limits are not exceeded. A maximum occupancy sign shall be conspicuously posted within the establishment and shall indicate the calculated occupant load, together with a simplified floor plan consistent with the Approved Project Plans.
- m. **Trash Disposal and Clean-Up:** Refuse and recycling receptacles shall be kept within the enclosure except during collection, in compliance with CMC Chapter 6.04. Trash disposal and routine exterior clean-up activities, including carpet cleaning, window cleaning, and sidewalk sweeping, shall not occur before 6:00 AM.

- n. **Smoking:** "No Smoking" signs shall be posted on the exterior of the establishment in compliance with CMC Section 6.11.060.
 - o. **Noise:** Regardless of decibel level or source, no noise generated by the establishment shall obstruct the free use of neighboring properties so as to unreasonably interfere with the comfortable enjoyment of the residents. In the event verified complaints are received by the City regarding such noise, the Community Development Director may immediately curtail the "business hours", "operational hours ", and/or "live entertainment hours", pursuant to Condition of Approval No. 6 (Revocation of Permit).
 - p. **Outdoor Storage:** No outdoor storage is permitted in association with the establishment, including within the parking lot, landscaped areas, sidewalks, or other exterior portions of the property.
 - q. **Lighting:** On-site lighting shall be shielded away from adjacent properties and directed on-site in compliance with the CMC Sec. 21.18.090 (Lighting Design Standards).
 - r. **Loitering:** There shall be no loitering allowed outside the establishment. The establishment owner is responsible for monitoring the premises to prevent loitering, including contacting the Police Department as necessary to enforce this requirement.
 - s. **Outdoor Seating:** Outdoor seating shall be limited to the existing outdoor patio approved under Building Permit No. BLD2019-00578. Additional outdoor seating is not authorized.
 - t. **City Meetings:** At the discretion of the Police Chief, periodic meetings will be conducted with representatives from the Police Department and the Department of Alcoholic Beverage Control (ABC) for ongoing employee training on alcoholic beverage service to the general public.
4. **Property Maintenance:** All exterior areas of the business shall be kept free from graffiti, trash, rubbish, and litter placed on the property, in compliance with CMC Section 21.16.110 (Site Maintenance).
5. **Landscape Maintenance:** All landscaping shall be maintained in compliance with Campbell Municipal Code Section 21.26.040 (Landscaping Maintenance Requirements) and Section 21.16.110.C.2 (Landscape Maintenance). Dead or unhealthy plants shall be replaced with healthy plants of the same or similar type.
6. **Revocation of Permit:** Exercise of this Approval is subject to Sections 21.68.020, 21.68.030 and 21.68.040 of the Campbell Municipal Code authorizing the appropriate decision making body to modify or revoke a Conditional Use Permit if it is determined that the sale of alcohol, live entertainment and/or late-night activities has become a nuisance to the City's public health, safety or welfare or for violation of this Approval or any standards, codes, or ordinances of the City of Campbell.

At the discretion of the Community Development Director, if the establishment generates three (3) verified complaints related to violations of conditions of approval

(e.g., noise, etc.) and/or related to the service of alcohol within a twelve (12) month period, a public hearing before the Planning Commission may be scheduled to consider modifying Conditions of Approval or revocation of the Approval. The Community Development Director may commence proceedings for the revocation or modification of the Approval upon the occurrence of less than three (3) verified complaints if the Community Development Director determines that the alleged violation warrants such an action. The Director may also at such time immediately restrict the approved "business hours", "operational hours", and/or "live entertainment hours" to address complaints in a timely manner.

In exercising this authority, the decision making body may consider the following factors, among others:

- a. The number and types of Police Department calls for service at or near the establishment that are reasonably determined to be a direct result of patrons actions;
- b. The number of complaints received from residents, business owners and other citizens concerning the operation of an establishment,
- c. The number of arrests for alcohol, drug, disturbing the peace, fighting and public nuisance violations associated with an establishment;
- d. The number and kinds of complaints received from the State Alcoholic Beverage Control office and the County Health Department; and
- e. Violation of conditions of approval.

Project Description for Effie's Live Entertainment Permit

We are requesting a Conditional Use Permit to allow for indoor live entertainment at our existing restaurant located at 331 Hacienda Avenue in Campbell, California. The proposed entertainment consists of karaoke, DJ performances, and occasional live bands, all of which take place within the existing bar area, using standard sound equipment and in compliance with City noise standards.

This establishment has operated at the same location since 1988, and karaoke has been an essential part of its identity for over three decades. For many years, karaoke and music have been hosted in the bar area and have operated smoothly. Today, as the new owners, we seek to bring this practice into full compliance with City regulations by obtaining the appropriate permit. The business was acquired by the current ownership in July 2024 with the goal of preserving its legacy and continuing to serve its dedicated customer base. The bar and restaurant are open seven days a week, with longer hours on weekends. Alcoholic beverages are served for on-site consumption in accordance with our ABC license.

Hours of Operation

Business Hours:

- 7:00 AM – 11:00 PM, Sunday through Tuesday
- 7:00 AM – 12:30 AM, Wednesday and Thursday
- 7:00 AM – 1:30 AM, Friday and Saturday

Operational Hours:

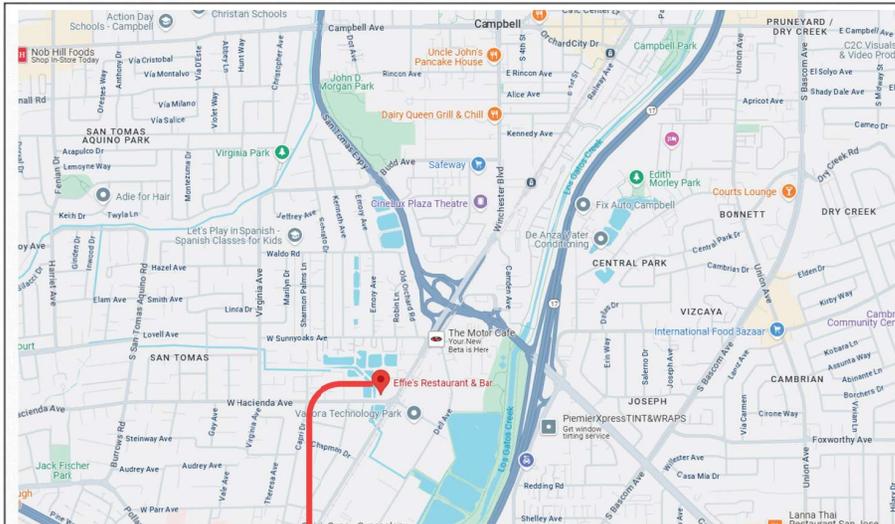
- 6:00 AM – 11:30 PM, Sunday through Tuesday
- 6:00 AM – 1:00 AM, Wednesday and Thursday
- 6:00 AM – 2:00 AM, Friday and Saturday

Live Entertainment Hours:

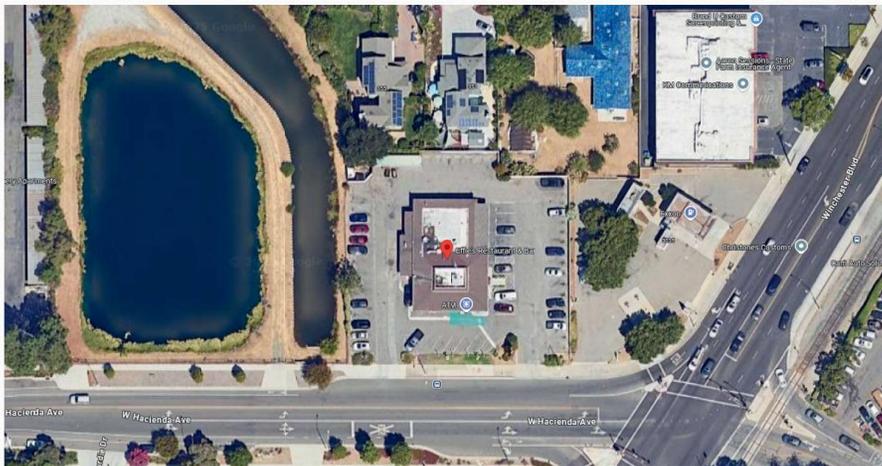
- Wednesday through Saturday: until 12:00 AM
- Sunday through Tuesday: until 11:00 PM

What makes Effie's special is the unique community that surrounds our business. The majority of our bar and karaoke patrons are longtime regulars, many of them over the age of 70, who have been visiting our restaurant for decades. For them, this place is more than just a restaurant or bar—it is a living archive of memories, friendships, and joyful moments. Many of our guests sing the same songs they sang 20 or 30 years ago, often celebrating birthdays, anniversaries, and reunions under the same roof.

Our goal is not to change the character of the establishment, but to preserve and protect it. We are not proposing any new construction, layout changes, or expansion—just the continued offering of the same musical and social experiences that have defined this venue for generations. We are deeply committed to maintaining a safe, respectful, and responsible environment that complies fully with City standards. At the same time, we recognize that this establishment serves as an important cultural and emotional space for the local community. In many ways, karaoke nights and live performances are not just entertainment—they are a way for people to stay engaged and connected to their community. Thank you very much for your consideration.



Project Location



There is no fire alarm system nor fire sprinkles.

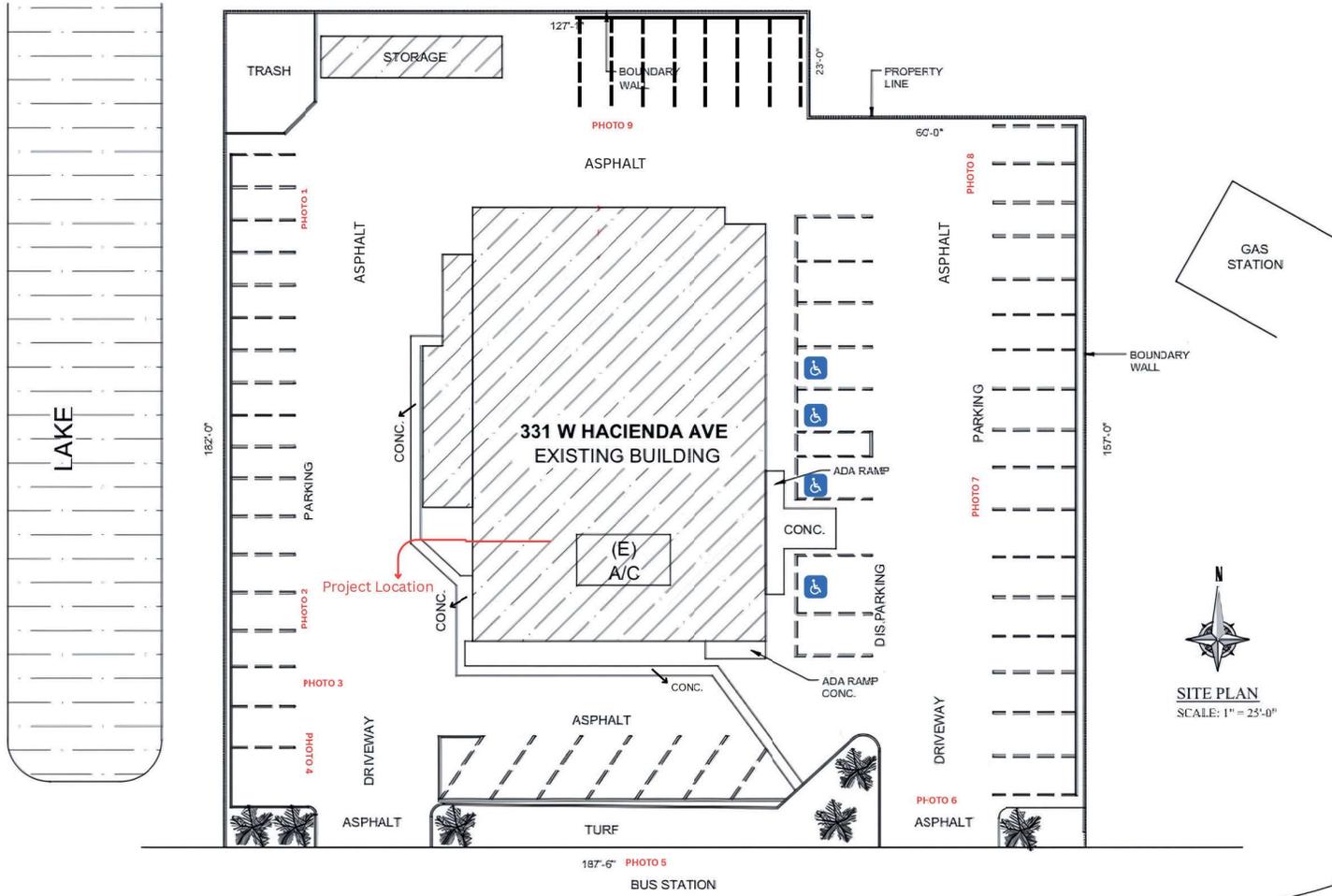
Project Info.

Property Owner	OLINGY DAVID J TRUSTEE
Project Address	331 W HACIENDA AVE, CAMPBELL, CA 95008
APN	40610007
Zoning Districts	GC
Scope of Work	live entertainment permit without any building changes
Parking	Standard=51, ADA= 4, Total=55
Lot Area	1(ac)/32234(sq ft)
Bldg. Floor Area	5,950 (sq ft)
FAR	5,950/32,234≈18.5 %
Landscaping Coverage	1274 (sq ft)

ADDRESS:
331 W HACIENDA AVE, CAMPBELL, CA 95008, USA

No.	Description	Date

COVER SHEET



W Hacienda Ave

ADDRESS:
331 W HACIENDA AVE, CAMPBELL, CA 95008, USA

No.	Description	Date

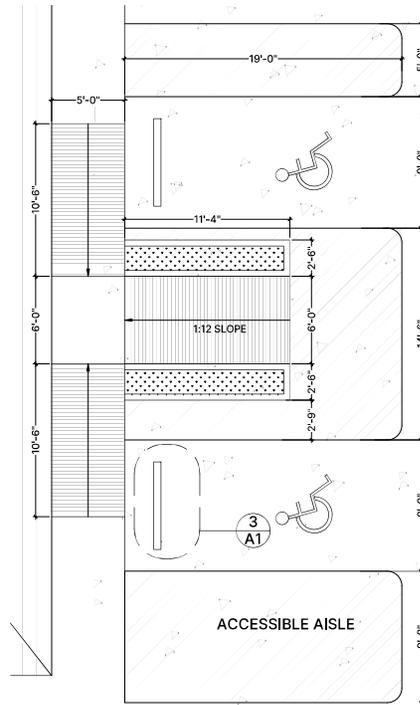
SITE PLAN

DATE: 3/10/23 PROJECT:
A 102

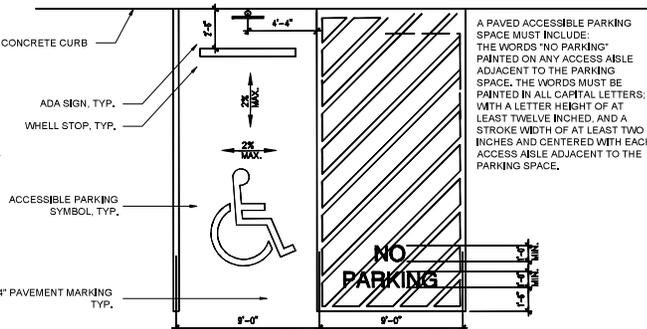
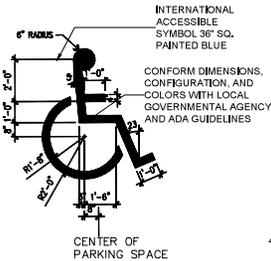
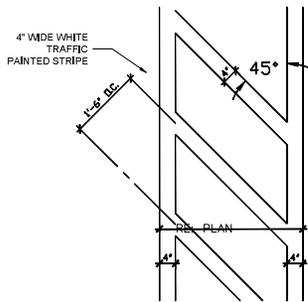


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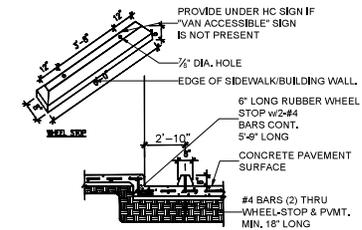
1 SITE PLAN
SCALE: 1/16" = 1'-0"



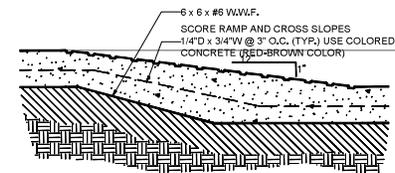
2 ENLARGE ADA PARKING PLAN
SCALE: 1/4" = 1'-0"



4 ACCESSIBLE ROUTE PAVEMENT MARKING SCALE: NTS 5 TYPICAL ADA PARKING STALL



3 CONCRETE WHEEL STOP DETAIL SCALE: NTS



6 ACCESSIBLE RAMP SECTION SCALE: NTS

EFFIE'S RESTAURANT & BAR
331 W HACIENDA AVE, CAMPBELL, CA 95008

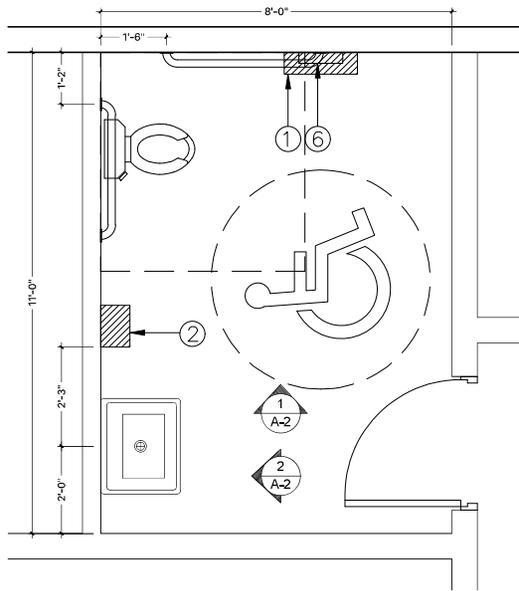
SITE PLAN

Project number: -
Date: 07.31.25
Drawn by: NH
Checked by:

A-1

Scale: VARIES

ARCH-D

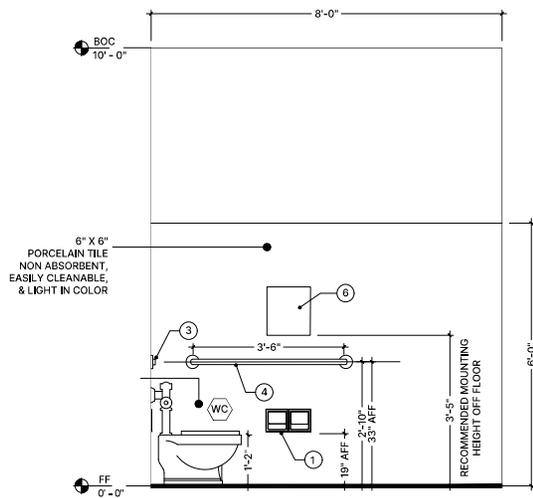


1 ENLARGED RESTROOM PLAN

SCALE : 3/4" = 1'-0"

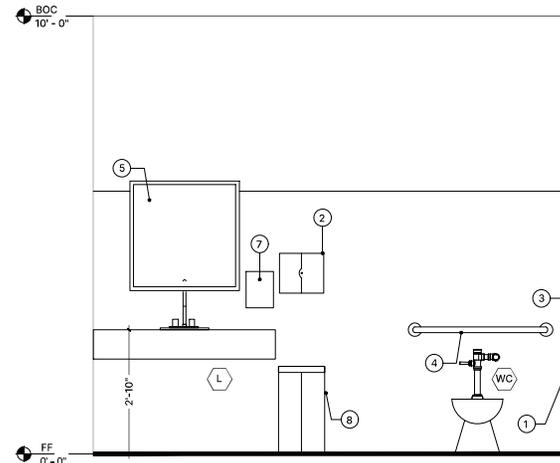
RESTROOM ACCESSORIES SCHEDULE						
MARK	QTY.	ACCESSORIES	MANUF.	MODEL/DESCRIPTION	MOUNTING HEIGHTS	REMARKS
①	1	DOUBLE ROLL TOILET TISSUE DISPENSER	GEORGIA PACIFIC	GPC59209	19" AFF	
②	1	MECHANICAL HANDS-FREE ROLL TOWEL DISPENSER	LOCOR	D68006-A BLACK	48" AFF	-
③	1	36" GRAB BAR	BOBRICK	B-6806 X 36"	33" AFF	SATIN FINISH, CONCEALED MOUNTING
④	1	42" GRAB BAR	BOBRICK	B-6806 X 42"	33" AFF	SATIN FINISH, CONCEALED MOUNTING
⑤	1	30"W x 30"H MIRROR	BOBRICK	B-165 1824	51" AFF	STAINLESS STEEL CHANNEL FRAME
⑥	1	SEAT COVER DISPENSER	GEORGIA PACIFIC	SAFE-T-GARD	48" AFF	RECESSED COLOR: SATIN FINISH
⑦	1	SOAP DISPENSER				
⑧	2	TRASH CANS				

EFFIE'S RESTAURANT & BAR
331 W HACIENDA AVE, CAMPBELL, CA 95008



2 RESTROOM INTERIOR ELEVATION

SCALE : 3/4" = 1'-0"



2 RESTROOM INTERIOR ELEVATION

SCALE : 3/4" = 1'-0"

ADA RESTROOM

Project number -
Date 07.31.25
Drawn by NH
Checked by

A-2

Scale AS NOTED



CITY OF CAMPBELL · PLANNING COMMISSION
Staff Report · October 28, 2025

PLN-2025-135
City-initiated

Public Hearing to consider recommendation of (1) an ordinance amending Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts) to facilitate housing development and clarify related regulatory requirements; and (2) an ordinance amending Chapter 21.20 (Density Bonuses and Other Housing Incentives) to implement Assembly Bill 1287 (2023) and establish a local density bonus incentive to encourage the production of small-scale "affordable by design" housing units in furtherance with the 2023–2031 Housing Element. File No.: PLN-2025-135.

STAFF RECOMMENDATION

That the Planning Commission take the following action:

- 1. Adopt a Resolution** (reference **Attachment 1**), recommending that the City Council adopt (1) an ordinance amending Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts) and (2) an ordinance amending Chapter 21.20 (Density Bonuses and Other Housing Incentives).

ENVIRONMENTAL DETERMINATION

Staff recommends that the Planning Commission accept the determination that these actions are exempt from the California Environmental Quality Act (CEQA) pursuant to Public Resources Code Section 21080.085, added by the SB 130 budget trailer bill as part of the State's recent CEQA streamlining reforms. This section applies to zoning changes implementing the schedule of actions in the City's adopted Housing Element under Government Code Section 65583(c). The actions are also exempt under CEQA Guidelines Section 15061(b)(3), as it can be seen with certainty that the ordinances will not result in a significant environmental effect because portions of the amendments implement existing State law.

BACKGROUND

On [May 27, 2025](#), the Planning Commission held a study session to receive a report and provide feedback on potential new housing policies and programs that would support housing production and affordability in Campbell. The report provided an overview of the City's Housing Element implementation and progress towards meeting the 6th Cycle (2023-2031) assigned Regional Housing Needs Allocation (RHNA).

Subsequently, on [August 19, 2025](#), City Council held a study session to receive the same report identifying potential new housing policies and programs that the City may pursue to support housing and affordable housing development in the City.

As discussed with the Planning Commission and City Council, the City is not currently on track to approve and permit sufficient housing to meet the City's assigned RHNA. It is anticipated that the City will produce approximately 690 units of its 2,977 RHNA by 2031, resulting in a shortfall of over 2,000 units. In addition, housing costs in Campbell have continued to remain high since the City's adoption of its 6th Cycle Housing Element in April 2023. The report identified current challenges in the housing market related to external factors (interest rates, construction costs, availability of affordable housing funding) and identified new programs the City could pursue in light of these challenges.

Based on Council's direction from the study session discussion, staff was asked to pursue implementation of the following actions:

- 1) Extend the entitlement period for 100% affordable housing projects from three to five years for qualifying projects under the City's Affordable Housing Overlay Zone (AHOZ).
- 2) Waive the ground floor commercial space requirements for 100% affordable housing projects under the AHOZ (outside of Downtown).
- 3) Allow housing projects with 5-9 units to satisfy their inclusionary housing requirements through payment of in-lieu fees (where previously only fractional obligations 0.5 and under could be satisfied through an in-lieu fee).
- 4) Standardize Accessory Dwelling Unit (ADU) requirements.

In response to Council's direction, staff has prepared draft amendments to three chapters of the Campbell Municipal Code:

- **Chapter 21.14** – Overlay/Combining Districts
- **Chapter 21.24** – Inclusionary Housing Ordinance
- **Chapter 21.20** – Density Bonuses and Other Housing Incentives

The proposed amendments address items 1-3 above, while item 4 will be implemented separately.

DISCUSSION

The Draft Ordinances introduce changes to the Inclusionary Housing Ordinance and the Overlay/Combining Districts (reference Attachment 1, Exhibit 1) and the Density Bonus Ordinance (reference Attachment 1, Exhibit 2). The proposed amendments include the key changes noted below. The first three bullet points implement the direction given by Council on new housing policies from the August 19, 2025, meeting, while the fourth bullet point acts to implement policy identified in the City's Housing Element. Further detail is provided below.

- **Extending the entitlement period** for 100% affordable housing projects under the AHOZ from three to five years. (Chapter 21.14 – Overlay/Combining Districts)
- **Waiving ground floor commercial requirements** for 100% affordable housing projects outside Downtown under the AHOZ. (Chapter 21.14 – Overlay/Combining Districts)
- **Allowing in-lieu fees** for projects with 5–9 units instead of requiring on-site affordable units. (Chapter 21.24 – Inclusionary Housing Ordinance)
- **Introducing a new local density bonus** for smaller units that are affordable by design. (Chapter 21.20 – Density Bonus Ordinance)

Chapter 21.14 (Overlay/Combining Districts)

The following amendments are proposed to enhance the effectiveness of the AHOZ:

- **Extension of Approval Period:** Extend the approval period for qualifying affordable housing AHOZ projects from three to five years. This change provides affordable housing developers with additional time to secure financing and permits, given current market challenges.
- **Ground Floor Commercial Space Exemption:** Exempt 100% affordable housing projects (outside Downtown) from the requirement to provide ground floor commercial space. Affordable Housing developers have indicated that this requirement makes it challenging to secure financing for ground floor commercial space associated with 100% affordable housing projects. The exemption would help such projects qualify for park impact fee reductions of up to 50% and other incentives offered under the AHOZ.

Chapter 21.24 (Inclusionary Housing Ordinance)

The proposed amendment to the Inclusionary Housing Ordinance will allow all 5–9-unit housing projects to pay an in-lieu fee to satisfy any whole or fractional unit obligation. This change would apply for a **five-year interim period**, ending June 30, 2030, prior to the start of the next Housing Element cycle.

This amendment is consistent with Housing Element Program H-1a, which encourages offering developers flexible compliance options to facilitate affordable housing production. In addition, this modification aligns with continued developer feedback citing financial feasibility concerns for the development of smaller housing projects. The change is also expected to increase revenues for the City’s Inclusionary Housing Trust Fund during the interim five-year period.

Chapter 21.20 (Density Bonuses and Other Housing Incentives)

The second ordinance establishes a new local density bonus program that incentivizes the construction of smaller, “affordable-by-design” housing units. Under the program, each qualifying “small-scale housing unit” provided within a housing development—defined as a studio or one-bedroom unit up to 625 square feet or a single-room occupancy unit up to 400 square feet—earns one additional bonus unit. To maintain housing diversity, the entire project would be subject to a project-wide average size cap of 1,250 square feet, consistent with Housing Element Program H-1d (i.e., the bonus units do not need to be small-scale).

The additional bonus units would also *not* be subject to the City’s 15% inclusionary housing requirement; however, the affordable inclusionary units would need to be distributed proportionally throughout the whole project, not just limited to the small units. This approach offers a straightforward incentive that developers can utilize, and the City can consistently administer, as shown in the example below.

Potential Density Increase by Share of Small Units (100-unit project)			
Scenario	25% Small Units	50% Small Units	100% Small Units
Base Units	100 (75 standard + 25 small)	100 (50 standard + 50 small)	100 (100 small)
Local Bonus Units	25	50	100
Total Units	125	150	200
% Increase Over Base	+25%	+50%	+100%

The program directly implements Housing Element Program H-1m, which calls for increasing residential densities by recognizing smaller unit types as a way to expand lower-cost housing choices in Campbell. While Program H-1m contemplated a “half-unit” counting methodology for density purposes, staff is recommending that the city instead to adopt a 1-to-1 bonus unit structure. This approach was made to provide greater clarity for applicants and staff, avoiding the complexities and unintended effects of fractional density calculations while achieving the same policy outcome of an overall increase in the number of smaller, more attainable housing units. The eligibility criteria for housing projects to utilize this program are listed below:

Eligibility Criteria	
1. Minimum density. The housing development has a minimum base density of 45 dwelling units per gross acre.	✓
2. Average unit size. The average unit size shall not exceed one thousand two hundred fifty (1,250) square feet.	✓
3. Affordability. Any units subject to Chapter 21.24 (Inclusionary Housing Ordinance) shall comply with all applicable requirements.	✓
4. Adherence to requirements. The housing development shall not use State Density Bonus Law, or any other State or Federal law, to relieve the project from any of the eligibility requirements.	✓

In addition to introducing this local program, the ordinance updates Chapter 21.20 of the Campbell Municipal Code to align with recent changes to State law, specifically Assembly Bill 1287 (2023). AB 1287 authorizes a “stacked” or “double” density bonus for qualifying projects, and this ordinance ensures that the City’s zoning regulations incorporate that new authority.

The proposed local small-scale housing bonus program may be used in combination with the State Density Bonus Law (SDBL) but does not have to be. The table in **Attachment 2** includes examples that show how the proposed program’s 1-to-1 bonus (one bonus unit granted for each small unit provided) could apply to a hypothetical one-acre property built at the minimum eligible density of 45 units per acre: (1) Base Density Only, (2) State SDBL Only, (3) Local Program Only, (4) Local + Standard SDBL, and (5) Local + SDBL + AB 1287. These examples illustrate how the programs can be combined to *maximize* housing production, but they may also be applied in more limited manner.

Together, the State updates and the local small-scale housing bonus strengthen the City’s toolkit for facilitating a wider range of housing opportunities consistent with the General Plan and adopted Housing Element.

NEXT STEPS

Following the subject meeting, the subject item will be heard by the City Council at its meeting of November 18, 2025.

ALTERNATIVES

1. Propose amendments to the Overlay/Combining Districts and Inclusionary Housing Ordinance, and/or the Density Bonus Ordinance policies or provisions as appropriate.

Prepared by: 
Eloiza Murillo-Garcia, Housing Program Manager

Prepared by: 
Stephen Rose, Senior Planner

Prepared by: 
Daniel Fama, Senior Planner

Approved by: 
Rob Eastwood, AICP, Community Development Director

Attachments:

1. Draft Resolution Recommending Approval of an Ordinance
 - (1) Ordinance Amending Chapter 21.24 and Chapter 21.14
 - (2) Ordinance Amending Chapter 21.20
2. Local and State Density Program Examples Table

RESOLUTION NO.

BEING A RESOLUTION OF THE PLANNING COMMISSION OF THE CITY OF CAMPBELL RECOMMENDING THAT THE CITY COUNCIL ADOPT AN (1) ORDINANCE AMENDING CHAPTER 21.24 (INCLUSIONARY HOUSING ORDINANCE) AND CHAPTER 21.14 (OVERLAY/COMBINING DISTRICTS) OF THE CAMPBELL MUNICIPAL CODE TO FACILITATE APPROVAL OF HOUSING DEVELOPMENT PROJECTS AND CLARIFY RELATED REGULATORY REQUIREMENTS; AND (2) AN ORDINANCE AMENDING CHAPTER 21.20 (DENSITY BONUSES AND OTHER HOUSING INCENTIVES) OF THE CAMPBELL MUNICIPAL CODE TO IMPLEMENT ASSEMBLY BILL 1287 (2023) AND ESTABLISH A LOCAL DENSITY BONUS INCENTIVE TO ENCOURAGE THE PRODUCTION OF SMALL-SCALE HOUSING UNITS IN FURTHERANCE WITH THE 2023–2031 HOUSING ELEMENT. FILE PLN-2025-135.

After notification and public hearing, as specified by law and after presentation by the Community Development Director, proponents and opponents, the hearing was closed.

The Planning Commission finds as follows with regard to the recommended approval of (1) an Ordinance amending Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts) and (2) an Ordinance amending Chapter 21.20 (Density bonuses and Other Housing Incentives):

1. Housing prices and rents in the City of Campbell have increased at a significantly higher rate than general wages. The lack of affordable housing in Campbell forces many residents to pay a very high percentage of their income for housing or to commute considerable distances, adding to air pollution and traffic congestion in Campbell and throughout Santa Clara County. The lack of affordable housing has made it more difficult to recruit workers from out of the area, in general, especially workers in lower-paying jobs, potentially affecting the economic vitality of the Campbell. New housing developments do not, to any appreciable extent, provide housing affordable to lower income households. As a result, there is a severe shortage of lower income housing as evidenced by the following:
 - a. According to the 2023-2031 Final Regional Housing Needs Allocation (RHNA) Plan for the San Francisco Bay Area, prepared by the Association of Bay Area Governments (ABAG), over half (56%) of the City's housing need is for very-low, low, and moderate-income households.
 - b. According to the U.S. Census Bureau, American Community Survey 5-Year Data (2015-2019) 44.6% of renters, and 27.4% of homeowners, in Campbell are cost burdened, spending more than 30% of their income on housing.
2. The proposed amendments to Chapter 21.24 (Inclusionary Housing Ordinance) of the Campbell Municipal Code include additional findings in Section 21.24.010 (Purpose and findings.) which are hereby included by reference.

3. The proposed amendments would be consistent with the 2040 General Plan and 2023-2031 Housing Element of the City of Campbell including, but not limited to, the following:

2040 General Plan

Goal LU-3: Employ creative approaches to providing a range of affordable housing options in Campbell while protecting the character of established neighborhoods.

LU-3.a: Implement the programs in the Housing Element in order to achieve the city's housing goals.

2023-2031 Housing Element

Program H-1a: Inclusionary Housing Ordinance Implementation: Amend the Inclusionary Housing Ordinance to offer developers a menu of options for achieving affordability, adjusting the percentage of units required to be affordable depending on the degree of affordability achieved (i.e., moderate-, low-, very low-, and extremely low income). These amendments could also include:

- Initiating a nexus study to reevaluate increasing the affordable housing in-lieu fee.*
- Updating the inclusionary ordinance to require for-sale projects to provide the same allocation of units available to lower-income and very low-income units that is required for rental projects.”*
- Modifying the In-Lieu Fee threshold to apply only to developments of 5-9 units. Housing developments of 10 or more units would not be allowed to substitute affordable units with in-lieu fees.*

4. The proposed amendments will implement Housing Element Program H-1m by incentivizing smaller unit sizes to provide lower-cost housing choices, which specifies that studios and one-bedroom units up to 625 square feet and single-room occupancy units up to 400 square feet be counted as one-half unit for density purposes. This ordinance achieves the same effect by granting one additional unit for each qualifying small unit, thereby encouraging production of smaller, affordable-by-design housing types.
5. Government Code Section 65915(n) authorizes local governments to adopt density bonus provisions that grant greater or proportionately lower density bonuses than those required by state law, and that this ordinance is adopted pursuant to that authority. Nothing in this ordinance is intended to reduce or otherwise constrain the rights afforded by Government Code Sections 65915 through 65918, and that the provisions herein are offered as additional, voluntary local incentives.
6. The proposed amendments will also implement Housing Element Program H-1d by reinforcing the average maximum unit size policy. Program H-1d establishes a 1,250-square-foot average maximum for projects exceeding 45 dwelling units per acre, and this ordinance incorporates that standard by limiting the size of bonus units.

7. Government Code Section 65915(n) authorizes local governments to adopt density bonus provisions that grant greater or proportionately lower density bonuses than those required by state law, and that this ordinance is adopted pursuant to that authority. Nothing in this ordinance is intended to reduce or otherwise constrain the rights afforded by Government Code Sections 65915 through 65918, and that the provisions herein are offered as additional, voluntary local incentives.
8. The proposed amendments will align Chapter 21.20 with recent amendments to state density bonus law enacted by Assembly Bill (AB) 1287 (2023), which authorizes an additional “stacked” density bonus for qualifying housing development projects.
9. The legislature of the State of California has, in Government Code Sections 65302, 65560 and 65800, conferred upon local governments the authority to adopt regulations designed to promote the public health, safety and general welfare of its citizenry.
10. The review and adoption of the proposed amendments is done in compliance with California Government Code Sections 65853 through 65857, which require a duly noticed public hearing of the Planning Commission whereby the Planning Commission shall provide its written recommendation to the City Council for its consideration.
11. Adoption of the proposed Ordinance is considered a "project" under Section 15378(a)(1) of the California Environmental Quality Act (CEQA).
12. The recommended adoption of an Ordinance may be found exempt from environmental review under Section 15061(b)(3) of the California Environmental Quality Act (CEQA) in that there is no possibility that recommending changes to the City’s Municipal Code may have a significant effect on the environment, and pursuant to Public Resources Code Section 21080.85, which applies to zoning changes implementing the schedule of actions in the City’s adopted Housing Element under Government Code Section 65583(c).

Based upon the foregoing findings of fact, the Planning Commission further finds and concludes that:

13. In consideration of the recommended adoption of the Ordinance, the Planning Commission provided due consideration of all evidence presented and provided in the entire administrative record.
14. The proposed amendments are consistent with the goals, policies, and actions of the 2040 General Plan and all applicable development agreements, area plans, neighborhood plans, and specific plans.
15. The proposed amendments will not be detrimental to the public interest, health, safety, convenience, or general welfare of the city.
16. The proposed amendments are internally consistent with other applicable provisions of the Campbell Municipal Code and Zoning Code.

17. No substantial evidence has been presented which shows that the project, as currently presented, will have a significant adverse impact on the environment.
18. The recommended adoption of an Ordinance is exempt under Section 15061(b)(3) of the California Environmental Quality Act (CEQA) as there is no possibility that the proposed changes to the City’s Municipal Code may have a significant effect on the environment.

THEREFORE, BE IT RESOLVED that the Planning Commission recommends that the City Council approve (1) an Ordinance amending Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts), as set forth in **Exhibit 1**; and (2) an Ordinance amending Chapter 21.20 (Density bonuses and Other Housing Incentives), as set forth in **Exhibit 2**.

PASSED AND ADOPTED this 28th day of October, 2025, by the following roll call vote:

AYES: Commissioners:
NOES: Commissioners:
ABSENT: Commissioners:
ABSTAIN: Commissioners:

APPROVED: _____
Matt Kamkar, Chair

ATTEST: _____
Rob Eastwood, Secretary

ORDINANCE NO.

BEING AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF CAMPBELL AMENDING CHAPTER 21.24 (INCLUSIONARY HOUSING ORDINANCE) AND CHAPTER 21.14 (OVERLAY/COMBINING DISTRICTS) OF THE CAMPBELL MUNICIPAL CODE TO FACILITY APPROVAL OF HOUSING DEVELOPMENT PROJECTS AND TO CLARIFY RELATED REGULATORY REQUIREMENTS. FILE NO. PLN-2025-135.

WHEREAS, after notification and public hearing, as specified by law and after presentation by the Community Development Director, proponents and opponents, the hearing was closed.

WHEREAS, the subject ordinance will amend Chapter 21.24 (Inclusionary Housing Ordinance) and Chapter 21.14 (Overlay/Combining Districts) of the Campbell Municipal Code and Chapter 21.14 (Overlay/Combining Districts) to facilitate approval of housing development projects and to clarify related regulatory requirements.

WHEREAS, housing prices and rents in the City of Campbell have increased at a significantly higher rate than general wages. The lack of affordable housing in Campbell forces many residents to pay a very high percentage of their income for housing or to commute considerable distances, adding to air pollution and traffic congestion in Campbell and throughout Santa Clara County. The lack of affordable housing has made it more difficult to recruit workers from out of the area, in general, especially workers in lower-paying jobs, potentially affecting the economic vitality of the Campbell. New housing developments do not, to any appreciable extent, provide housing affordable to lower income households. As a result, there is a severe shortage of lower income housing as evidenced by the following:

- a. According to the 2023-2031 Final Regional Housing Needs Allocation (RHNA) Plan for the San Francisco Bay Area, prepared by the Association of Bay Area Governments (ABAG), over half (56%) of the City's housing need is for very-low, low, and moderate-income households.
- b. According to the U.S. Census Bureau, American Community Survey 5-Year Data (2015-2019) 44.6% of renters, and 27.4% of homeowners, in Campbell are cost burdened, spending more than 30% of their income on housing.

WHEREAS, the proposed amendments to Chapter 21.24 (Inclusionary Housing Ordinance) of the Campbell Municipal Code include additional findings in Section 21.24.010 (Purpose and findings.) which are included herein by reference.

WHEREAS, the proposed amendments would be consistent with the 2040 General Plan and 2023-2031 Housing Element of the City of Campbell including, but not limited to, the following:

2040 General Plan

Goal LU-3: Employ creative approaches to providing a range of affordable housing options in Campbell while protecting the character of established neighborhoods.

LU-3.a: Implement the programs in the Housing Element in order to achieve the city's housing goals.

2023-2031 Housing Element

Program H-1a: Inclusionary Housing Ordinance Implementation: Amend the Inclusionary Housing Ordinance to offer developers a menu of options for achieving affordability, adjusting the percentage of units required to be affordable depending on the degree of affordability achieved (i.e., moderate-, low-, very low-, and extremely low income). These amendments could also include:

- Initiating a nexus study to reevaluate increasing the affordable housing in-lieu fee.*
- Updating the inclusionary ordinance to require for-sale projects to provide the same allocation of units available to lower-income and very low-income units that is required for rental projects.”*
- Modifying the In-Lieu Fee threshold to apply only to developments of 5-9 units. Housing developments of 10 or more units would not be allowed to substitute affordable units with in-lieu fees.*

WHEREAS, the legislature of the State of California has, in Government Code Sections 65302, 65560 and 65800, conferred upon local governments the authority to adopt regulations designed to promote the public health, safety and general welfare of its citizenry.

WHEREAS, the review and adoption of the proposed amendments is done in compliance with California Government Code Sections 65853 through 65857, which require a duly noticed public hearing of the Planning Commission whereby the Planning Commission shall provide its written recommendation to the City Council for its consideration.

WHEREAS, adoption of the proposed Ordinance is considered a "project" under Section 15378(a)(1) of the California Environmental Quality Act (CEQA).

WHEREAS, the proposed Ordinance may be found exempt from environmental review under Section 15061(b)(3) of the California Environmental Quality Act (CEQA) in that there is no possibility that the proposed changes to the City's Municipal Code may have a significant effect on the environment.

WHEREAS, the legislature of the State of California has, in Government Code Sections 65302, 65560 and 65800, conferred upon local governments the authority to adopt regulations designed to promote the public health, safety and general welfare of its citizenry.

WHEREAS, the proposed amendments will not be detrimental to the public interest, health, safety, convenience, or general welfare of the city.

WHEREAS, the proposed amendments are internally consistent with other applicable provisions of the Campbell Municipal Code and Zoning Code.

WHEREAS, in consideration of the adoption of the subject Ordinance, the City Council provided due consideration of all evidence presented and provided in the entire administrative record.

THEREFORE, the City Council of the City of Campbell does ordain as follows:

Section 1. Chapter 21.24 (Inclusionary Housing Ordinance) of the Campbell Municipal Code is hereby amended to read as set forth in **Exhibit A** with underlining (underlining) indicating new text and strikeouts (~~strikeouts~~) indicating deleted text.

Section 2. Section 21.14.070.C.4. (Mixed-use development.) of the Campbell Municipal Code is hereby amended to read as follows with underlining indicating new text and strikeouts (~~strikeout~~) indicating deleted text:

4. Mixed-use development. Where the City's adopted Form-Based Zone Map designates a "Tier 1" ground-floor non-residential requirement, the project must adhere to all corresponding regulations outlined in the adopted Multi-Family Development and Design Standards, established in accordance with Campbell Municipal Code section 21.07.030 (Multi-Family Development and Design Standards Document and Form-Based Zone Map adopted). This eligibility standard shall not apply to housing development projects that meet the criteria of Government Code Section 65915(b)(1)(G) and are located outside of the "Downtown Development Plan" boundary depicted on General Plan Figure LU-2 (Special Planning Areas); and

Section 3. Section 21.14.070.D.1. (Increased density bonus.) of the Campbell Municipal Code is hereby amended to read as follows with underlining indicating new text and strikeouts (~~strikeout~~) indicating deleted text:

1. Increased density bonus. The following density bonuses shall be offered to projects which provide a greater percentage of very-low, low-, or moderate-income housing units, by category, than otherwise required by State Density Bonus Law but less than required to qualify for an additional density bonus under state law. In no case shall the combination of the density granted under this Section and any other law result in a total density bonus that is greater than seventy percent, except for bonus unit granted for providing small-scale housing units in accordance with Campbell Municipal Code Section 21.20.060.B.3. (Density bonus for providing small-scale housing units). Further, density bonuses provided for by this Section may not be combined across income categories and shall be calculated consistent with Campbell Municipal Code Section 21.20.050.A (Number of units) excepting that for Step 2 - the corresponding percentage density bonus shall be calculated based on the values indicated in the tables below.

Section 4. Section 21.14.070.D.3. (Extended permit approval.) of the Campbell Municipal Code is hereby amended to read as follows with underlining indicating new text and strikeouts (~~strikeout~~) indicating deleted text:

3. Extended permit approval. Projects qualifying for an increased density bonus of any amount, in accordance with CMC Section 21.14.070.D.1., shall be granted a ~~three~~five-year permit approval for the first approval of all land use entitlements permits. The permit approval period of post-entitlement permits, such as building permits, and/or land use entitlements granting subsequent extensions of approval, and/or modifications to the first land use approval, shall not be extended as an incentive of this Section.

Section 5. If any section, sentence, clause, phrase, word, or other provision of this Ordinance is for any reason held to be unconstitutional or otherwise invalid, such holding shall not affect the validity of the remaining sections, sentences, clauses, phrases, words or other provisions of this Ordinance, or the validity of this Ordinance, shall stand notwithstanding the invalidity of any section, sentence, clause, phrase, word or other provision.

Section 6. The proposed Ordinance is exempt under Section 15061(b)(3) of the California Environmental Quality Act (CEQA) as there is no possibility that the proposed changes to the City’s Municipal Code may have a significant effect on the environment.

Section 7. The City Council further finds and determines that the proposed Ordinance is consistent with the goals, policies, and actions of the 2040 General Plan and the 2023-2031 Housing Element.

Section 8. That this Ordinance shall become effective thirty (30) days following its passage and adoption and shall be published, or summary thereof, one time within fifteen (15) days upon passage and adoption in the Metro Silicon Valley, a newspaper of general circulation for the City of Campbell, County of Santa Clara.

PASSED AND ADOPTED this _____ day of _____, 2025, by the following roll call vote:

AYES: COUNCILMEMBERS:
NOES: COUNCILMEMBERS:
ABSENT: COUNCILMEMBERS:
ABSTAIN: COUNCILMEMBERS:

APPROVED: _____
Sergio Lopez, Mayor

ATTEST: _____
Andrea Sanders, City Clerk

Chapter 21.24 INCLUSIONARY HOUSING ORDINANCE¹

21.24.010 Purpose and findings.

- A. The purpose of this Chapter is to enhance the public welfare by establishing policies which facilitate the availability of housing affordable to a broad range of households with varying income levels within the City of Campbell, meet the City's regional share of housing needs, and implement the goals and objectives of the General Plan and Housing Element.
- B. The City Council desires to encourage the development of inclusionary units to be located upon the same site as market rate residential development to provide for integration of lower income households with households in market rate neighborhoods and to disperse Inclusionary Units throughout the City where new residential development occurs.
- C. The City's Housing Element recognizes that affordable housing is a critical component to the City's economic and social health. Establishing policies which facilitate the availability of housing affordable to a broad range of households with varying income levels within the City of Campbell is consistent with and will implement the City's Housing goals, policies, and programs for Affordable Housing which include, but are not limited to, the following:
 - 1. Goal 1: Improve Housing Affordability in Campbell - Encourage the Production of Affordable Housing; and
 - 2. Program H-1a (Inclusionary Housing Ordinance Implementation).
- D. Providing affordable housing for new employees in lower income jobs locally (rather than leaving those employees to obtain housing in lower cost areas far from Campbell employment centers and transit corridors) helps minimize housing's contribution to greenhouse gas emissions and locating housing to reduce vehicle miles traveled and auto dependency. If the increased demand for affordable housing is not addressed within Campbell it will cause such housing to be built elsewhere, in areas with lower land values that are far from city employment centers and transit corridors, and the resulting commuting will cause increased traffic and transit demands and consequent noise and air pollution.
- E. If the increased demand for affordable housing is not addressed within Campbell it may also impact economic growth in that businesses within the city may find it more difficult to attract and retain the variety of workers that they need.

¹Ord. No. 2318, § 1(Exh. A), adopted Sep. 3, 2024, repealed the former Ch. 21.24, §§ 21.24.010—21.24.100, and enacted a new Ch. 21.24 as set out herein. The former Ch. 21.24 pertained to similar subject matter and derived from Ord. 2074, Att. 3 (part), adopted 2006; Ord. No. 2206, § 14, adopted Aug. 2, 2016; Ord. No. 2252, § 20, adopted Nov. 19, 2019; Ord. No. 2293, § 1(Exh. B), adopted May 2, 2023.

- F. Establishing policies which facilitate the availability of housing affordable to a broad range of households with varying income levels also implements policies of the State of California to: (1) provide sufficient capacity for new housing affordable at all income levels necessary to accommodate the State's future economic growth; and (2) by providing housing for lower income workers, long commutes from less expensive housing markets can be avoided and thus contribute to the implementing the Global Warming Solutions Act of 2006 and the Sustainable Communities and Climate Protection Act of 2008.
- G. State housing policy requires the City to assist in the development of adequate housing to meet the needs of lower income households. There is a significant shortage of housing affordable to lower income households, which will only increase as the finite number of residentially zoned parcels within the city are purchased and developed for market rate residential developments. This loss of residential land available for affordable housing is another impact of market rate residential development.
- H. The requirements of this Chapter are based on a number of factors including, but not limited to, the City's long-standing commitment to economic diversity; the serious need for affordable housing as reflected in local and State housing laws, regulations, and policies; the demand for affordable housing created by the development of market rate housing; and the impact that the lack of affordable housing production has on the health, safety, and welfare of the City's residents including its impacts on traffic, transit and related air quality impacts, and the demands placed on the regional transportation infrastructure. Establishing policies which facilitate the availability of housing affordable to a broad range of households with varying income levels that are reasonably related to the burdens created by new market-rate development on the City's need for affordable housing will contribute to addressing impacts and fulfilling goals of the City and State of California.
- I. The City also desires to provide the residential development community with alternatives to construction of the inclusionary units on the same site as the market rate residential development. Therefore, Section 21.24.070 (Alternatives) includes a menu of options from which an applicant may elect, in lieu of building affordable units within a residential project. Nothing in this Chapter shall deem or be used to deem the compliance options in Section 21.24.070 (Alternatives), including, but not limited to, the in lieu housing fee authorized pursuant to Section 21.24.70.C., as an ad hoc exaction, as a mandated fee required as a condition to developing property, or as a fee subject to the analysis in *Building Industry Association of Central California v. City of Patterson* (5th Dist. 2009) 171 Cal. App. 4th 886.
- J. The City has prepared a Nexus Study which shows that there is a reasonable relationship between the purpose for which the in-lieu housing fees established by this Chapter are to be used and the type of development projects on which the fees are imposed, and between the need for affordable housing and market-rate development and between the amount of the fees and the cost of the affordable housing units or portion of the units attributable to the development on which the fees are imposed. The Nexus Study complies with the requirements of California

Government Code section 66001(A)(4) by establishing the reasonable relationship between the need for affordable housing and the development of market-rate housing.

- K. It is the intent of the City that the in-lieu housing fee established by this Chapter shall be supplementary to any conditions imposed upon a market-rate housing project pursuant to other provisions of the Municipal Code, the Subdivision Map Act, the California Environmental Quality Act, and other State and local laws, which may authorize the imposition of project specific conditions on development.
- L. High housing costs relative to income can contribute to overcrowding which has been found to contribute to impacts to public health, safety, and welfare according to the U.S. Department of Housing and Urban Development's 2007 report on "Measuring Overcrowding in Housing" and the Center for Disease Control's 2020 publication on "Health Effects of Housing."
- M. The requirements of this Chapter are adopted pursuant to the City's police power authority to protect the public health, safety, and welfare. The requirements of this section are consistent with Housing Element goals related to protecting the public welfare by fostering an adequate supply of housing for persons at all economic levels and maintaining both economic diversity and geographically dispersed affordable housing.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.020 Applicability.

This chapter shall not apply to rental projects or for-sale projects that are developed in accordance with, and subject to, a recorded governmental agency deed restriction or regulatory agreement that imposes affordable rent, occupancy, and eligibility restrictions or affordable ownership cost and sale price restrictions with associated eligibility and occupancy restrictions on all residential units in the development, except for manager's units. Such deed restriction or regulatory agreement must be recorded against the property, be binding on successors in interest for at least the same duration as required by this chapter, and be executed by the property owner and any of the following entities or agencies: the City of Campbell, the County of Santa Clara, the California Tax Credit Allocation Committee, the issuer of tax-exempt bonds used to finance the residential development, the California Department of Housing and Community Development or any other State of California agency, and the U.S. Department of Housing and Urban Development or any other federal agency.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.030 Definitions.

As used in this chapter, the following terms shall have the meanings set forth below:

"Appliances" means refrigerators, stovetops, ranges, microwaves, air conditioners, furnaces, heat pumps, electric vehicle chargers, whole house fans, and battery storage systems.

"Affordable ownership cost" means average monthly housing costs during the first calendar year of a household's occupancy, as determined by the city, including mortgage payments, loan insurance fees, if any, property taxes, reasonable allowances for utilities and property maintenance and repairs, homeowners insurance and homeowners association dues, if any, that is affordable to a low- or moderate-income household, or at even lower levels of affordability, adjusted for household size.

"Affordable rent" means monthly rent, including utilities and all fees for housing services, which do not exceed thirty percent of the gross monthly income adjusted for household size.

"Affordable unit(s)" means one or more living units which are required under this chapter to be rented at affordable rents or available at an affordable housing cost to specified households.

"Applicant" means a person or entity who applies for a residential project and, if the applicant does not own the property on which the residential project is proposed, also means the owner or owners of the property.

"Area median income" means area median income for Santa Clara County as published pursuant to California Code of Regulations, Title 25, Section 6932 (or its successor provision).

"Construction cost index" means the California Construction Cost Index (CCCI) published by the California Department of General Services, Real Estate Service Division. If that index ceases to exist, the community development director shall substitute another construction cost index which in his or her judgment is as nearly equivalent to the original index as possible.

"Common ownership" means real property owned or controlled by the same person, persons, or entity, or by separate entities in which any shareholder, partner, member, or family member of an investor of the entity owns ten percent (10%) or more of the interest in the real property."

"Development project" means any project undertaken for the purpose of development. "Development project" includes a project involving the issuance of a permit for construction or reconstruction but not a permit to operate. "Development project" does not include any ministerial projects proposed to be carried out or approved by public agencies.

"Eligible household" means a household whose household income does not exceed the maximum specified in Section 21.24.040 (General requirements.) of this chapter for a given affordable unit.

"Evidence of common ownership" includes, but is not limited to, a contract, covenant, easement, lease, license, or similar document that either:

(a) vests majority control over the management, use, development, or transfer of an interest in one or more contiguous lots in Common Ownership that have majority control

over the management, use, development, or transfer of an interest in another contiguous lot; or

(b) aligns the management, use, development, or transfer of an interest in one or more contiguous lots with the management, use, development, or transfer of an interest in another contiguous lot all in Common Ownership, or makes the management, use, development, or transfer of an interest in one or more contiguous lots dependent on the management, use, development, or transfer of an interest in another contiguous lot all in Common Ownership.

Exception: Notwithstanding the foregoing, lots shall not be deemed to be in common ownership where the property owner(s) submit a sworn affidavit, in a form acceptable to and approved by the City Attorney, demonstrating to the City's satisfaction that the lots are not subject to common ownership or control as otherwise defined by this section.

"Extremely low-income household" means a household whose household income does not exceed thirty percent of the area median income in Santa Clara County, as published and periodically updated by the State Department of Housing and Community Development pursuant to Section 50105 of the California Health and Safety Code.

"Fee Satisfied Affordable Unit Obligation" means any affordable housing unit obligation to be satisfied by payment of an in-lieu housing fee in accordance with Campbell Municipal Code section 21.24.070.C. (In-Lieu Fee).

"First approval" means the first of the following approvals to occur with respect to a residential project: building permit, tentative parcel map, tentative subdivision map, conditional use permit, site and architectural review permit, or other discretionary city land use approval.

"For-sale project" means a residential project, or portion thereof, which is intended to be sold to owner-occupants upon completion.

"Household income" means the combined adjusted gross income for all adult persons living in a living unit as calculated for the purpose of the Section 8 Program under the United States Housing Act of 1937, as amended, or its successor.

"Initial Affordable Unit Obligation " means the total calculated affordable housing unit obligation of a residential project as required by Section 21.24.040 (General requirements.) before application of the requirement to construct the next higher whole number of affordable units.

"Inclusionary housing agreement" means an agreement between the city and an applicant, governing how the applicant shall comply with this chapter.

"Livable area" means the total square footage of the interior of all dwelling units within a residential development that a tenant can occupy and use as measured from the interior face of the exterior walls. This area specifically includes all living spaces, kitchens and bathrooms, and usable closet and storage spaces within a dwelling unit. This definition specifically excludes common areas such as hallways, elevators, stairwells, mailrooms, multi-use rooms, indoor pool rooms, mechanical rooms, lobbies,

parking areas, and areas shared with a non-residential use in a mixed-use development.

"Living unit" means one or more rooms designed, occupied, or intended for occupancy as separate living quarters, with cooking, sleeping, and bathroom facilities. For the purposes of this Chapter, Accessory Dwelling Units (ADUs) and Junior Accessory Dwelling Units (JADUs) shall not count as living units.

"Lower-income" means a household whose household income is less than eighty percent of the area median income in Santa Clara County, as published and periodically updated by the California Department of Housing and Community Development.

"Low-income household" means a household whose household income is greater than fifty percent and does not exceed eighty percent of the area median income in Santa Clara County, as published and periodically updated by the California Department of Housing and Community Development pursuant to Section 50079.5 of the California Health and Safety Code.

"Manager's unit" means a housing unit within a rental housing development that is designated for occupancy by a property manager or on-site staff responsible for the management and maintenance of the property.

"Market rate unit" means a housing unit or the legal lot for such unit offered on the open market at the prevailing market rate for purchase or rental.

"Ministerial project" means a project for which the governmental agency's action involves only the application of fixed, objective standards or criteria, without personal judgement or discretion. Ministerial projects do not require discretionary approval by a public agency.

"Moderate-income household" means a household whose household income is greater than eighty percent and does not exceed one hundred twenty percent of the area median income in Santa Clara County, as published and periodically updated by the California Department of Housing and Community Development pursuant to Section 50093 of the California Health and Safety Code.

"Nexus Study" means the fee study entitled: "Affordable Housing Nexus Study" prepared by BAE Urban Economics, dated July 15, 2024, approved by the City Council on August 20, 2024, and on file with the City Clerk.

"Rental project" means a residential project, or portion thereof, which is intended to be rented to tenants upon completion.

"Residential project" ~~has two distinct meanings depending on the date of application submittal as follows:~~

~~1. For a residential project submitted less than one year after the effective date of this Chapter, "Residential project" means, any parcel map, subdivision map, conditional use permit, site and architectural review permit, building permit, or other city approval, which authorizes ten or more living units or residential lots, or living units and residential lots with ten or more in combination., exclusive of any proposed accessory dwelling units. In order to prevent evasion of the provisions of this chapter, contemporaneous construction of ten or more living units on a lot, or on contiguous lots for which there is~~

~~evidence of common ownership or control, even though not covered by the same city land use approval, shall also be considered a residential project. Construction shall be considered contemporaneous for all units which do not have completed final inspections for occupancy and which have outstanding, at any one time, any one or more of the following: parcel map, subdivision map, or other discretionary city land use approvals, or building permits, or applications for such an approval or permits.~~

~~2. For a residential project submitted one year or more after of the effective date of this Chapter, "Residential project" means, any parcel map, subdivision map, conditional use permit, site and architectural review permit, building permit, or other city approval, which authorizes five or more living units or residential lots, or living units and residential lots with five or more in combination, exclusive of any proposed accessory dwelling units. In order to prevent evasion of the provisions of this chapter, contemporaneous construction of five or more living units on a lot, or on contiguous lots for which there is evidence of common ownership or control, even though not covered by the same city land use approval, shall also be considered a residential project. Construction shall be considered contemporaneous for all units which do not have completed final inspections for occupancy and which have outstanding, at any one time, any one or more of the following: parcel map, subdivision map, or other discretionary city land use approvals, or building permits, or applications for such an approval or permits.~~

~~3. "Very low-income household" means a household whose household income is greater than thirty percent and does not exceed fifty percent of the area median income in Santa Clara County, as published and periodically updated by the State Department of Housing and Community Development pursuant to Section 50105 of the California Health and Safety Code.~~

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.040 General requirements.

A. Percentage requirement. At least fifteen percent of all living units, excluding any density bonus units and accessory dwelling units, in a residential project shall be made available at affordable rents or affordable ownership cost as prescribed in this section and shall be approved and completed not later than the times prescribed in Section 21.24.050 (Time performance.), unless an alternative mode of compliance is approved as set forth in Section 21.24.070 (Alternatives.), of this chapter. When the percentage requirement results in a fraction of units, the owner of the property shall ~~round fractional units as follows:~~

~~1. For a residential project submitted less than one year after the effective date of this Chapter, the property owner shall construct the next higher whole number of affordable units where the fraction is 0.5 or greater and shall construct the next lower whole number of affordable units where the fraction of units is less than 0.5.~~

~~2. For a residential project submitted one year or more after the effective date of this Chapter, for any fractions of units in residential projects the owner of the property shall construct the next higher whole number of affordable units.~~

- B. Provision of affordable units. Where affordable and market rate units are provided in a residential project, the affordable units, irrespective of income level, shall be indistinguishable, in terms of building materials, architectural features, lighting fixtures, type and number of appliances, and landscaping areas from other residential units in the residential project. Further, affordable units, irrespective of income level, shall have the same access to parking facilities, project amenities, and recreational areas as other residential units in the residential project.
- C. Location and distribution of affordable units. Where affordable and market rate units are provided in a residential project, the affordable units shall be distributed proportionally to the total units in the residential project, inclusive of any density bonus units, in terms of their assignment to rental and ownership units, and their placement in buildings, floors, and model types (accounting for floor plans, bedroom count, and unit sizes) where different buildings, floors, or model types are present in the residential project.
- D. For-sale projects. Affordable units which are constructed in for-sale projects for owner-occupancy shall be sold at affordable ownership cost to low-income households and moderate-income households. Of these affordable units in for-sale projects, forty percent of the required fifteen percent, or six percent of the total units in the residential project, shall be offered at affordable ownership costs exclusively to low-income households, provided that where this requirement for low-income units would result in a fraction of a low-income unit, the number of low-income units shall be rounded up and the number of moderate-income units which need not be low-income units shall be rounded down. For determining compliance with the affordability requirements of this section, units provided at even lower income levels than those specified shall be found to satisfy the requirement. The sale price of units shall be based on the methodology established by, and on file with, the Community Development Department consistent with the requirements of this Chapter.
- E. Rental projects. The affordable units which are constructed in rental projects shall be offered for rent at affordable rents to low-income households and very low-income households. Of these affordable units in rental projects, forty percent of the required fifteen percent, or six percent of the total units in the residential project, shall be offered at affordable rents exclusively to very low-income households, provided that where this requirement for very low-income units would result in a fraction of a very low-income unit, the number of very low-income units shall be rounded up and the number of low-income units which need not be very low-income units shall be rounded down. For determining compliance with the affordability requirements of this section, units provided at even lower income levels than those specified shall be found to satisfy the requirement. The rental rates of units shall be based on the methodology established by, and on file with, the Community Development Department consistent with the requirements of this Chapter.
- F. Priority for rental or purchase of units. Preference in the rental or purchase of affordable units shall be given, first to income eligible lower income households of protected units displaced by demolition for any required replacement units as

provided for by Section 66300.6 of the California Government Code, second (for up to ten percent of all affordable units subject to this chapter) to income eligible employees of the City of Campbell, third to income eligible existing Campbell residents, and fourth to income eligible persons employed within the city limits of the City of Campbell.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.050 Time performance.

- A. Application requirements. An application for first approval of a residential project will not be deemed complete until the applicant has submitted plans and proposals which demonstrate the manner in which the applicant proposes to meet the requirements of this chapter, including any plans for the construction of on-site units pursuant to Section 21.24.040 (General requirements.) of this chapter or the applicant's selection of an alternative mode of compliance pursuant to Section 21.24.070 (Alternatives.) of this chapter.
- B. Conditions. Conditions to carry out the purposes of this chapter shall be imposed on the first approval for a residential project. Additional conditions may be imposed on later city approvals or actions, including without limitation tentative parcel maps, tentative subdivision maps, conditional use permits, housing development permits, site and architectural review permits, or building permits. The conditions of approval included with the first approval of the residential project shall further provide that prior to the recordation of the parcel map or final map in the case of subdivisions and or prior to the issuance of building permits in the case of all other land use permits to which this chapter applies, the applicant shall enter into an inclusionary housing agreement acceptable to the community development director that contains specific requirements implementing the condition of approval including, but not limited to, as applicable, the number of affordable units, the level(s) of affordability, location and type of affordable units, timing of construction of affordable units in relation to the construction of the market rate units contained in the development, preferences given in selecting occupants, and amount of the in-lieu fee, if any. The inclusionary housing agreement may be amended by the parties, provided the amendment is consistent with the condition of approval imposed as part of the first approval and the then-existing city approvals. If such proposed amendment is minor or technical in nature, the community development director shall have authority to approve or disapprove the amendment on behalf of the city. If such proposed amendment makes a substantive or material change to the inclusionary housing agreement, such amendment shall be effective only if, following notice and hearing and such other procedures as may be required by law, approved by the city agency that gave the first approval on the project.
- C. No building permit shall be issued for any market rate unit until the applicant has obtained permits for affordable units sufficient to meet the requirements of Section 21.24.040 (General requirements.) of this chapter, or received approval of an alternative requirement of Section 21.24.070 (Alternatives.) of this chapter. No final inspection for occupancy for any market rate unit shall be completed until the

applicant has constructed the affordable units required by Section 21.24.040 (General requirements.) of this chapter, or completed corresponding alternative performance under Section 21.24.070 (Alternatives.) of this chapter. The time requirements set forth in this subsection for issuance of building permits for market rate units and for final inspections for occupancy for market rate units may be modified to accommodate phasing schedules, model variations, or other factors in a residential project, if the city determines this will provide greater public benefit and an inclusionary housing agreement acceptable to the community development director or the Community Development Director's designee pursuant to subsection B of this section so provides.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.060 Continued affordability and city review of occupancy.

- A. Term of affordability—For-sale projects. A resale restriction, covenant, deed of trust and/or other documents acceptable to the community development director or the director's designee, shall be recorded against each affordable owner-occupied unit. These documents shall, in the case of affordable units which are initially sold, be for a term of forty-five years and shall be renewed at the change of each title for a period of forty-five years for a residential project submitted less than one- year from the effective date of this Chapter. For a residential project submitted one year or more after the effective date of this Chapter, the term shall be fifty-five years and shall be renewed at the change of each title for a period of fifty-five years. The resale restriction, or other documents authorized by this subsection, and any change in the form of any such documents which materially alters any policy in the documents, shall be approved by the community development director or his or her designee prior to being executed with respect to any residential project.
- B. Term of affordability—Rental projects. A regulatory agreement, covenant, deed of trust, and/or other documents acceptable to the Community Development Director or the Director's designee, shall be recorded against each unit/complex for residential projects containing affordable rental units. These documents shall, in the case of affordable units which are rented, be for a term of fifty-five years and shall be renewed at the change of each title for a period of fifty-five years. The regulatory agreement and other documents authorized by this subsection, shall run with the property and not be affected by the sale of the property or units in the project. The regulatory agreement and other documents authorized by this subsection, and any change in the form of any such document which materially alters any policy in the document, shall be approved by the Community Development Director or his or her designee prior to being executed with respect to any residential project.
- C. Eligibility requirements. No household shall be permitted to begin occupancy of an affordable unit unless the city or its designee has approved the household's eligibility. If the city or its designee maintains a list of, or otherwise identifies, eligible households, initial and subsequent occupants of affordable units shall be selected first from the list of identified households, to the maximum extent possible, in

accordance with rules approved by the community development director or his or her designee.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.070 Alternatives.

An applicant may elect, in lieu of building affordable units within a residential project, to satisfy the requirements of this chapter by one of the following alternative modes of compliance, provided that the applicant includes such election in its application for the first approval of the residential project and that the criteria stated in the relevant subsection below are satisfied.

- A. Off-site construction. Construct, or make possible construction by another developer of, units not physically contiguous to the market-rate units (or units that are physically contiguous to the market-rate units if the City determines this will provide greater public benefit and if an inclusionary housing agreement acceptable to the Community Development Director or his or her designee pursuant to Section 21.24.050(B) (Conditions.) of this chapter so provides) and equal or greater in number to the number of affordable units required under Section 21.24.040 (General requirements.) of this chapter. Off-site construction pursuant to this subsection shall be approved only if:
 1. Approval has been secured for the off-site units not later than the time the residential project is approved and completion of the off-site units is secured by a requirement that final inspections for occupancy for the related market-rate units be completed after those for the affordable units, provided that the time requirements set forth in this subsection for final inspections for occupancy for market-rate units may be modified to accommodate phasing schedules, model variations, financing requirements, or other factors in a residential project for the off-site units, if the City determines this will provide greater public benefit, and if an inclusionary housing agreement acceptable to the Community Development Director or his or her designee pursuant to Section 21.24.050(B) (Conditions.) of this chapter so provides;
 2. The off-site units will be greater in number, larger or affordable to households with lower incomes than would otherwise be required in Section 21.24.040 (General requirements.) of this chapter;
 3. Financing or a viable financing plan is in place for the off-site units;
 4. In the event the off-site units receive any public assistance, the developer of the residential project will contribute to the off-site units economic value equivalent to the value of making on-site units in the developer's residential project affordable; and
 5. The City may require that completion of off-site units shall be further secured by the developer's agreement to pay an in-lieu fee in the amount

due under subsection D of this section in the event the off-site units are not timely completed.

- B. Land dedication. Dedicate without cost to the city, a lot or lots within or contiguous to the residential project, sufficient to accommodate at least the required affordable units for the residential project. An election to dedicate land in lieu of compliance with other provisions of this chapter shall be allowed only if:
1. The value of the lot or lots to be dedicated is sufficient to make development of the otherwise required affordable units economically feasible, and financing or a viable financing plan is in place for at least the required number of affordable units; and
 2. The lot or lots are suitable for construction of affordable housing at a feasible cost, served by utilities, streets and other infrastructure, there are no hazardous material or other material constraints on development of affordable housing on the lot or lots, and land use approvals have been obtained as necessary for the development of the affordable units on the lot or lots.
- C. In-lieu housing fee. The applicant of a residential project ~~submitted one year or more after the effective date of this Chapter~~ may elect to pay an in-lieu housing fee, in accordance with Section 21.24.080 (Establishment and use of in-lieu housing fees), instead of developing the affordable units required in Section 21.24.040 (General requirements.) of this ~~C~~chapter, pursuant to the requirements set forth below:
1. Projects with ~~five-nine or fewer or six~~ units:
 - a. ~~Effective through June 30, 2030, the applicant of a housing development project with nine or fewer living units. The applicant may pay an in-lieu housing fee to satisfy any whole or fractional affordable unit obligation required by this Chapter (i.e., 0.75 or 0.9 units) rather than constructing the next higher whole number of affordable units.~~
 - a.b. ~~Effective July 1, 2030, the applicant of a housing development project with nine or fewer living units may pay an in-lieu housing fee to satisfy any fractional housing unit obligation required by this Chapter (i.e., 0.75, or 0.9) rather than constructing the next higher whole number of affordable units.~~
 2. Projects with ~~more than nine seven or more~~ units: The applicant ~~of a housing development project with nine or fewer living units~~ may pay an in-lieu housing fee to satisfy any fractional affordable unit obligation less than 0.5 rather than constructing the next higher whole number of affordable units. Nothing contained in this section shall alleviate the applicant from developing the remainder of the affordable units required by Section 21.24.040 (General requirements) or allow for payment of an in-lieu housing fee in situations where the fractional obligation is 0.5 or greater.

3. Calculation of in-lieu fees. In-lieu fees shall be calculated based on the fee schedule:

- a. In effect at the time the fee is paid for a ministerial project application;
- b. The date an application was deemed complete for a development project application; or
- c. The date the complete preliminary application was submitted for a project submitted pursuant to California Government Code Section 65941.1.

If a permit meets more than one of the above criteria, the fee shall be based on the fee schedule in effect on the date that results in the lowest fee from the list above.

In-lieu fees shall be paid prior to issuance of building permits. If building permits are issued for only part of a residential project, the fee amount shall be based only on the number of units then permitted.

- D. Substitution of units. The applicant may elect to provide additional affordable units, instead of developing the affordable units required in Section 21.24.040 (General requirements.) of this chapter, as set forth below:
 - 1. Very Low-Income Units: The applicant may substitute required very low-income housing units at a ratio of one to two for low-income units or at a ratio of one to four for moderate income units.
 - 2. Low-Income Units: The applicant may substitute required low-income housing units at a ratio of one to two for moderate income units.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.080 Establishment and use of in-lieu housing fees.

- A. Initial in-lieu housing fee. The initial in-lieu housing fee shall be set by City Council resolution based on an affordable housing analysis or nexus study which supports the initial fee rate and fee calculation method established in this Section.
- B. Fee calculation method. The in-lieu housing fee shall be calculated as follows:

$$\text{In-Lieu Fee} \times \frac{\text{Fee Satisfied Affordable Unit Obligation}}{\text{Initial Affordable Unit Obligation}} \times \text{Livable Area} = \text{Total Due}$$
~~$$\text{Fee} \times \frac{\text{Fractional Affordable Units}}{\text{Total Affordable Units Required}} \times (\text{Livable Area}) = \text{Total Due}$$~~

- C. Fee adjustments. The initial fee established, in accordance with Section 21.24.080.A. (Initial in-lieu housing fee.), shall be adjusted annually on July 1, based on the percentage change in the construction cost index from the previous year.
- D. All in-lieu fees collected under this chapter shall be deposited into a separate account to be designated the City of Campbell housing trust fund.

- E. The in-lieu fees collected under this chapter and all earnings from investment of the fees shall be expended exclusively to provide or assure continued provision of affordable housing in the city through acquisition, construction, development assistance, rehabilitation, financing, rent subsidies or other methods, and for costs of administering programs which serve those ends. The housing shall be of a type, or made affordable at a cost or rent, for which there is a need in the city and which is not adequately supplied in the city by private housing development in the absence of public assistance.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.090 Waiver of requirements.

Notwithstanding any other provision of this chapter, the requirements of this chapter shall be waived, adjusted or reduced if the applicant shows that there is no reasonable relationship between the impact of a proposed residential project and the requirements of this chapter, or that applying the requirements of this chapter would take property in violation of the United States or California Constitution or otherwise result in an unconstitutional application of this chapter. To receive a waiver, adjustment or reduction under this section, the applicant must file a written request together with the development application(s) when applying for a first approval for the residential project, and/or as part of any appeal which the City provides as part of the process for the first approval. The written request shall provide substantial evidence showing that there is no reasonable relationship between the impact of a proposed residential project and the requirements of this chapter, or that applying the requirements of this chapter would take property in violation of the United States or California Constitution or otherwise result in an unconstitutional application of this Chapter. The City may assume that: (a) the applicant will provide the most economical inclusionary units feasible meeting the requirements of this Chapter; and (b) the applicant is likely to obtain housing subsidies when such funds are reasonably available. The waiver, adjustment, or reduction may be approved only to the extent necessary to avoid an unconstitutional result, after adoption of written findings, based on substantial evidence, supporting the determinations required by this section.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.100 Enforcement.

- A. The City Council may adopt guidelines, by resolution, to assist in the implementation of all aspects of this Chapter.
- B. No permit, license, subdivision approval or map, or other approval or entitlement for a residential project shall be issued, including without limitation a final inspection for occupancy or certificate of occupancy, until all requirements applicable to the residential project at such time pursuant to this Chapter have been satisfied.
- C. The City Attorney shall be authorized to enforce the provisions of this Chapter and all inclusionary housing agreements, regulatory agreements, resale controls, deeds

of trust, or similar documents placed on affordable units, by civil action and any other proceeding or method permitted by law.

- D. Failure of any official or agency to fulfill the requirements of this Chapter shall not excuse any applicant or owner from the requirements of this Chapter.
- E. The remedies provided for in this Chapter shall be cumulative and not exclusive and shall not preclude the City from any other remedy or relief to which it otherwise would be entitled under law or equity.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.110 Phased projects.

- A. Purpose. This section serves to ensure that each development phase of a multi-phase development project contributes its fair share of affordable units and maintains consistency with the overall project's requirements.
- B. Required Affordable Units by Project Phase. Each phase of a development project must provide at least as many affordable units, by income level, as would be required if that phase, along with any previously completed project phase(s), were completed independently from the rest of the project.
- C. Building Permit Final. No development phase of a project shall receive a building permit final until all affordable units required by that phase of the project are completed.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

21.24.120 Severability.

If any provision or clause of this Chapter or the application thereof is held to be invalid, such invalidity shall not affect the other provisions or applications of this Chapter which can be given effect without the invalid provision or application, and to this end, the provisions of this Chapter are declared to be severable.

(Ord. No. 2318, § 1(Exh. A), 9-3-2024)

Ordinance No. _____

BEING AN ORDINANCE OF THE CITY COUNCIL OF THE CITY OF CAMPBELL AMENDING CHAPTER 21.20 (DENSITY BONUSES AND OTHER HOUSING INCENTIVES) OF THE CAMPBELL MUNICIPAL CODE TO IMPLEMENT ASSEMBLY BILL 1287 (2023) AND ESTABLISH A LOCAL DENSITY BONUS INCENTIVE TO ENCOURAGE THE PRODUCTION OF SMALL-SCALE HOUSING UNITS IN FURTHERANCE OF PROGRAM H-1M OF THE 2023–2031 HOUSING ELEMENT. FILE NO. PLN-2025-135

After notification and public hearing, as specified by law and after presentation by the Community Development Director, proponents and opponents, the hearing was closed.

After due consideration of all evidence presented, the City Council of the City of Campbell does ordain as follows:

SECTION 1. The City Council finds that this ordinance implements Housing Element Program H-1m by incentivizing smaller unit sizes to provide lower-cost housing choices, which specifies that studios and one-bedroom units up to 625 square feet and single-room occupancy units up to 400 square feet be counted as one-half unit for density purposes. This ordinance achieves the same effect by granting one additional unit for each qualifying small unit, thereby encouraging production of smaller, affordable-by-design housing types.

SECTION 2. The City Council further finds that this ordinance implements Housing Element Program H-1d by reinforcing the average maximum unit size policy. Program H-1d establishes a 1,250-square-foot average maximum for projects exceeding 45 dwelling units per acre, and this ordinance incorporates that standard by limiting the size of bonus units.

SECTION 3. The City Council further finds that Government Code Section 65915(n) authorizes local governments to adopt density bonus provisions that grant greater or proportionately lower density bonuses than those required by state law, and that this ordinance is adopted pursuant to that authority. Nothing in this ordinance is intended to reduce or otherwise constrain the rights afforded by Government Code Sections 65915 through 65918, and that the provisions herein are offered as additional, voluntary local incentives.

SECTION 4. The City Council further finds that this ordinance aligns Chapter 21.20 with recent amendments to state density bonus law enacted by Assembly Bill (AB) 1287 (2023), which authorizes an additional “stacked” density bonus for qualifying housing development projects.

SECTION 5. The City Council further finds and determines that this ordinance is statutorily exempt from the California Environmental Quality Act (CEQA) pursuant to Public Resources Code Section 21080.85, which applies to zoning changes implementing the schedule of actions in the City’s adopted Housing Element under Government Code Section 65583(c), and CEQA Guidelines Section 15061(b)(3), as it can be seen with certainty that the ordinance will not result in a significant environmental effect because it implements existing State law, AB 1287.

SECTION 6. The City Council further finds and determines that the proposed ordinance is consistent with the goals, policies, and actions of the General Plan; would not be detrimental to the public interest, health, safety, convenience, or general welfare of the city; and is internally consistent with other applicable provisions of the Zoning Code.

SECTION 7. Section 21.20.020 (Definitions) of the Campbell Municipal Code is hereby amended to add the following definition between the terms "Affordable rent" and "Base units" to read as follows with underlining indicating new text:

"Affordable units" has the same meaning as "target units" in this Chapter.

SECTION 8. Section 21.20.020 (Definitions) of the Campbell Municipal Code is hereby amended to add the following definition between the terms "Percentage density bonus" and "Target Units" to read as follows with underlining indicating new text:

"Small-scale housing units" means studio and one-bedroom living units up to six hundred twenty-five (625) square feet of livable area, and single room occupancy (SRO) units up to four hundred (400) square feet of livable area. For the purposes of this definition, "livable area" shall have the same meaning as provided in Section 21.24.030 (Definitions), except that it shall refer to the interior livable area of each individual dwelling unit.

SECTION 9. Section 21.20.040.B.1.a. of the Campbell Municipal Code is hereby amended to read as follows with underlining indicating new text and ~~strikeout~~ indicating deleted text:

- a. Summary table showing the maximum number of dwelling units permitted by the zoning and general plan (~~excluding any density bonus units~~), proposed affordable units by income level, proposed bonus percentage, number of density bonus units proposed, number of small-scale housing units (if applicable), total number of dwelling units proposed on the site, and resulting density in units per acre;

SECTION 10. Section 21.20.050.A (Number of units) of the Campbell Municipal Code is hereby amended to read as follows with underlining indicating new text and ~~strikeout~~ indicating deleted text:

- A. Number of units. In determining the total number of bonus units to be granted, each component of any density calculation, including base density and bonus density, resulting in fractional units shall be separately rounded up to the next whole number. When calculating the number of affordable units needed for a given density bonus, any fractions of affordable dwelling units shall be rounded up to the next whole number. Density bonus provided under Government Code Section 65915 and any additional density bonus available under Government Code Section 65915(v) shall each be calculated with reference to the base units, independently of one another, and the resulting bonus amounts combined to determine the total number of allowable density bonus units. Any additional density bonus authorized by this Chapter shall be calculated separately and added to the State density bonus units. ~~The allowable density bonus is computed as follows:~~

~~Step 1.— Calculate the percentage that the number of "target units" is of the total base units, rounding up to the next whole number;~~

~~Step 2.— Using the percentage calculated in Step 2, identify the corresponding percentage density bonus from California Government Code sections 65915 through 65918;~~

~~Step 3.— Determine the number of allowable density bonus units by multiplying the percentage density bonus times the maximum allowable base units, and rounding up to the next whole number.~~

SECTION 11. Section 21.20.050.B of the Campbell Municipal Code is hereby amended to read as follows with underlining indicating new text and ~~strikeouts~~ indicating deleted text:

- B. Each housing development is entitled to only one density bonus, except as provided in Government Code Section 65915(v) or in this Chapter. If a housing development qualifies for a density bonus under more than one income category or additionally for the category of housing to be provided, the applicant shall select the category under which the density bonus is granted. Density bonuses from more than one category may not be combined.

SECTION 12. Section 21.20.050 (Calculation) of the Campbell Municipal Code is hereby amended to include a new subsection 'F' with underlining indicating new text:

F. The number of base units shall be rounded down to the next whole number in accordance with Section 21.02.020.D (Residential density) before adding the number of allowable bonus units. The resulting sum establishes the maximum number of housing units permitted for the housing development project.

SECTION 13. Section 21.20.060.B (Local incentives) of the Campbell Municipal Code is hereby amended to include a new subsection '3' with underlining indicating new text:

3. Density bonus for providing small-scale housing units. In accordance with Housing Element Program H-1m and pursuant to Government Code Section 65915(n), housing development projects incorporating small-scale housing units shall receive a local density bonus, as specified herein, when meeting all the requirements of this section.

a. Project Eligibility. Housing development projects seeking a density bonus for providing small-scale housing units shall satisfy all the following requirements:

i. Minimum density. The housing development project shall have a minimum base density of 45 dwelling units per gross acre.

ii. Average unit size. The average unit size of the housing development project shall not exceed one thousand two hundred fifty (1,250) square feet, consistent with Housing Element Program H-1d. The average unit size shall be calculated based on the livable area of all dwelling units, including bonus units.

iii. Affordability. The housing development project shall comply with all applicable requirements of Chapter 21.24 (Inclusionary Housing Ordinance), including distributing the below-market-rate (BMR) units proportionally throughout the entire housing development, inclusive of all density bonus units, consistent with Section 21.24.040.C (Location and distribution of affordable units), except that the density bonus units granted under this subsection shall not themselves be subject to the fifteen percent (15%) affordability requirement.

- iv. Adherence to requirements. The housing development shall not use State Density Bonus Law, or any other State or Federal law, to relieve the project from any of the requirements of this section.
- b. Bonus units. For each one (1) small-scale housing unit among the base units provided, one (1) density bonus unit shall be granted. The number of bonus units granted pursuant to this subsection shall not exceed the number of base units in the housing development project.
- c. Combination. A density bonus granted under this subsection may be combined with the density bonus authorized by Government Code Section 65915(f), the additional stacked bonus authorized by Section 65915(v), and the density bonus authorized by Section 21.14.070 (Affordable Housing overlay/combining zoning district). Bonus units granted pursuant to this subsection shall not be counted as base units for purposes of State Density Bonus Law or for any other purpose.
- d. Maintenance. Small-scale housing units used to obtain bonus units shall remain configured as such for a minimum of fifty-five (55) years from issuance of certificate of occupancy. The applicant shall record a covenant, in a form approved by the City Attorney, prohibiting unit mergers or enlargements that would exceed the size limits of a small-scale housing unit, as defined by Section 21.20.020 (Definitions), during this period.
- e. Non-waiver. Compliance with the eligibility, bonus, affordability, and unit-size requirements of this subsection is a condition of receiving the local density bonus. No waiver of the provisions of this subsection shall be required pursuant to Government Code Section 65915, and no incentive or concession shall be granted for the purpose of modifying the requirements of this subsection.

SECTION 14. The local density bonus established by this ordinance may be exercised by an applicant for any housing development application that was accepted for filing, deemed complete, or the subject of a preliminary application pursuant to Government Code Section 65941.1 before the effective date of this ordinance. The Community Development Director may apply this bonus to an approved housing development application if the conditions of approval authorize administrative adjustments to unit counts consistent with this ordinance, provided no increase to the approved building envelope (height, stories, floor area, or building footprint) is required. Any change to the approved building envelope (height, stories, floor area, or building footprint) shall require an amendment to the approved housing development application by the original decision-making body.

SECTION 15. If any section, subsection, sentence, clause, or phrase of this ordinance is for any reason held to be invalid or unconstitutional by a decision of any court of competent jurisdiction, such decision shall not affect the validity of the remaining portions of the ordinance. The City Council hereby declares that it would have passed this ordinance and each and every section, subsection, sentence, clause, or phrase not declared invalid or unconstitutional without regard to whether any portion of the ordinance would be subsequently declared invalid or unconstitutional.

SECTION 16. This ordinance shall become effective thirty (30) days following its passage and adoption and shall be published, one time within fifteen (15) days upon passage and adoption in a newspaper of general circulation in the City of Campbell, County of Santa Clara.

PASSED AND ADOPTED this _____ day of _____, 2025 by the following roll call vote:

AYES: Councilmembers:
NOES: Councilmembers:
ABSENT: Councilmembers:

APPROVED:

Sergio Lopez, Mayor

ATTEST:

Andrea Sanders, City Clerk

Local and State Density Program Examples Hypothetical 1-Acre Lot in High Density Mixed-Use Zoning District (34-45 units/acre)						
Scenario Description	Base Units (1)	Local Bonus Units (2)	SDBL Bonus Units (3)	AB 1287 Bonus Units (4)	Total Units (5)	Affordable Units (6)
1. Base Density Only No use of local or state density bonus.	45	0	0	0	45	7
2. State SDBL Only Standard SDBL bonus only (up to 50%)	45	0	23	0	68	7
3. Local Program Only Local Small Unit Density Bonus applied <i>without</i> SDBL. (3)	45	45	0	0	90	7
4. Local + Standard SDBL Local bonus combined with standard SDBL (up to 50%).	45	45	23	0	113	7
5. Local + SDBL + AB 1287 Local bonus combined with standard SDBL and AB 1287 stacked bonus (up to 100%).	45	45	23	23	136	14
Notes						
(1) The proposed program requires a minimum density of 45 units per acre.						
(2) Assumes all 45 of the "base units" are small-scale units satisfying the definition.						
(3) Assumes a 50% bonus for projects with at least 15% very low-income units (Gov. Code §65915(f)), which can be satisfied by increasing the affordability of the otherwise required low- and moderate-income inclusionary units.						
(4) Assumes a second 50% bonus for projects providing an <i>additional</i> 15% moderate-income units (Gov. Code §65915(v)).						
(5) Fractional units from each component (base, state, local, AB 1287) are rounded up (Gov. Code §65915(q)).						
(6) Number of affordable units shown in aggregate and not distinguished by income level for illustrative purposes.						



CITY OF CAMPBELL · PLANNING COMMISSION
Staff Report · October 28, 2025

File No(s): Study Session on Metropolitan Transportation Commission (MTC)
PLN-2023-138 Transit-Oriented Communities (TOC) Policy.

RECOMMENDATION

That the Planning Commission receive the report and recommend to the City Council appropriate next steps regarding the city's conformance with MTC's Transit-Oriented Communities (TOC) Policy, including, but not limited to, supporting actions to provide better conformance, or taking no further action.

PURPOSE

The purpose of this Study Session is to evaluate the City of Campbell's alignment with the Metropolitan Transportation Commission's (MTC) Transit-Oriented Communities (TOC) Policy, adopted in May 2025, and to consider its potential implications for the Hamilton Avenue corridor. This evaluation fulfills the City's commitment under the Priority Development Area (PDA) Planning Grant, which funded the Hamilton Avenue Public Improvement Plan, to consider compliance with TOC policies as part of the corridor planning process. While adoption of the TOC Policy is not required, greater alignment with TOC principles can advance the City's goals of enhancing multimodal connectivity and promoting housing, economic, and mixed-use redevelopment, and may strengthen the City's competitiveness for future regional grant opportunities.

The report summarizes key elements of the TOC Policy, assesses the City's current level of compliance, and identifies actions the City could take to achieve greater conformance and enhance its competitiveness for future One Bay Area Grant (OBAG 4) funding opportunities. It also provides an analysis of the TOC Policy in relation to recent amendments to state law.

OVERVIEW

The Metropolitan Transportation Commission (MTC) is the regional transportation planning, financing, and coordinating agency for the nine-county San Francisco Bay Area. As the federally designated Metropolitan Planning Organization (MPO), MTC is responsible for developing the Regional Transportation Plan/Sustainable Communities Strategy (RTP/SCS), allocating federal and state transportation funds, and establishing policies that guide regional investment decisions.

In May 2025, MTC adopted the Transit-Oriented Communities (TOC) Policy to promote more sustainable, equitable, and transit-supportive development patterns across the Bay Area. The TOC Policy is intended to:

- Encourage higher-density housing and employment near major transit corridors.

- Improve multimodal access to and within transit areas.
- Support mixed-use and inclusive community development.
- Advance regional climate, housing, and equity goals by reducing vehicle miles traveled (VMT) and increasing access to transit.

MTC established the TOC Policy in part to link regional land use and transportation objectives with local planning and development decisions. Local jurisdictions, including the City of Campbell, are not mandated to adopt the TOC Policy in full. However, a jurisdiction's consistency with TOC standards may influence its eligibility and competitiveness for regional transportation and infrastructure funding programs, such as the One Bay Area Grant (OBAG).

The City of Campbell participated in the development of the TOC Policy and provided feedback on the draft procedural guidance and scoring framework. The City's correspondence emphasized the need for flexibility in residential and office density standards, a phased approach to compliance with the option for per-station scoring, and reduced weighting for satisfying standards that are already required under state law. The City also cautioned against parking maximums that could constrain redevelopment feasibility and encouraged MTC to align the TOC scoring framework with the State's Prohousing Designation Program to promote greater consistency and equity across jurisdictions (reference **Attachment 1** – City Feedback on TOC Scoring Framework).

DISCUSSION

The Transit-Oriented Communities (TOC) Policy consists of four policy elements including: 1) Residential and Commercial Office Density; 2) Affordable Housing Production; 3) Parking Management; and 4) Transit Station Access. Within these four elements are a total of eleven (11) standards and requirements intended to promote transit-oriented development. This report provides a concise summary of each policy area, the applicable standards or requirements, and staff's analysis of the City's current level of compliance or consistency. Where gaps are identified, the report also identifies staff recommendations and options for future City action in response.

Key sections of the report, including the Access Gap Analysis (Policy Area #10) and Mobility Hub Planning (Policy Area #11) were prepared with assistance of the City's Hamilton Avenue Public Improvement Plan consultant team led by Mithun. In addition, coordination with MTC and VTA was undertaken separately to support the preparation of this report and to inform the City's planned policies and implementation actions.

In applying these policies, it is important to note that the TOC standards are not citywide—they apply only within one-half mile of qualifying public transit stations and corridors. The Metropolitan Transportation Commission (MTC) scores each jurisdiction's consistency with the TOC Policy across all station areas collectively, rather than on a station-by-station basis. While this is the required methodology the staff analysis also examines performance at the individual station level where appropriate to better visualize variations in policies and land use patterns. This dual approach provides a more detailed understanding to help inform future policy and land use decisions.

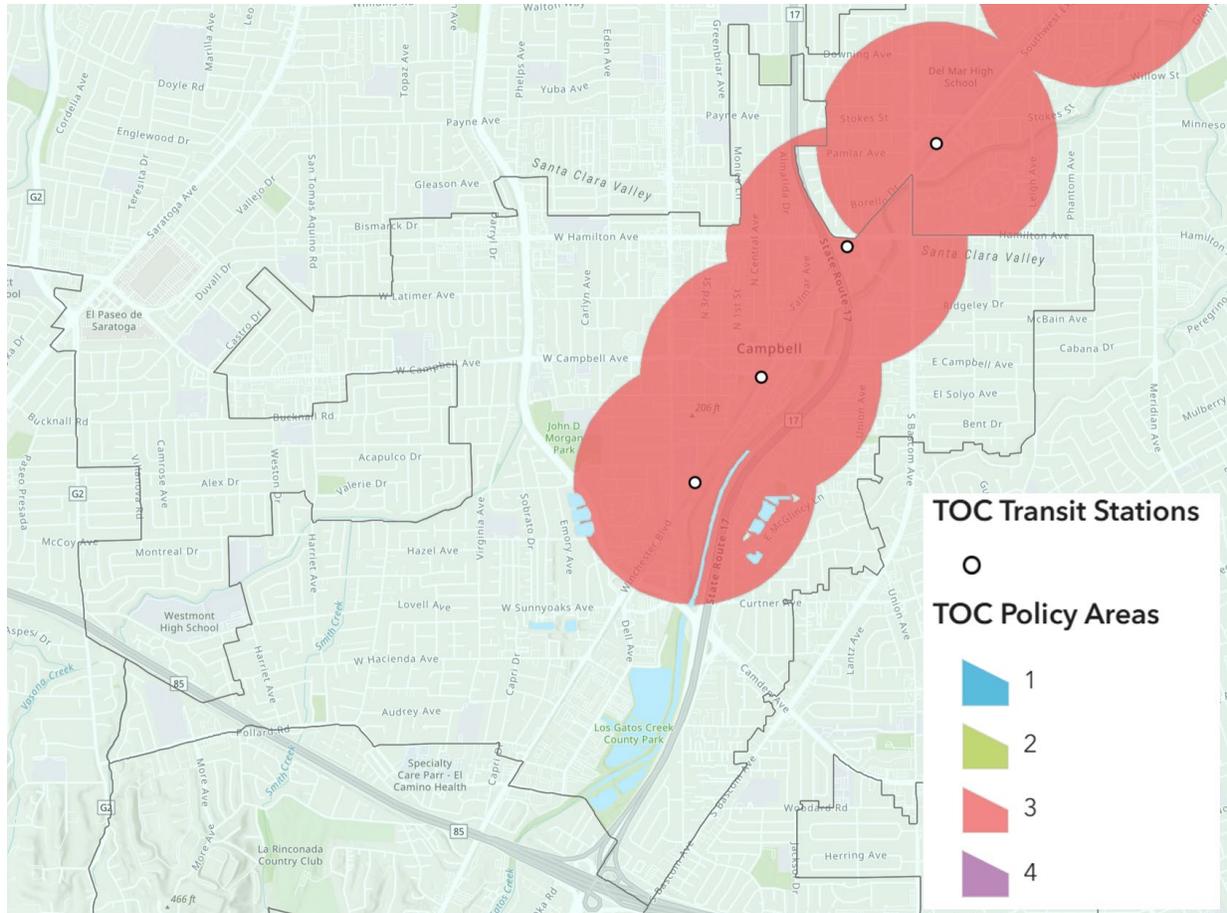


Figure 1 – TOC Transit Stations & Policy Area Tiers

The Transit-Oriented Communities (TOC) Policy is structured around two distinct tiering systems.

First, the policy standards and requirements vary based on the service tier of the applicable transit stop. All of Campbell’s eligible transit stops fall within Tier 3 Areas¹, and therefore, the standards and requirements applying to Tier 3 Areas are those evaluated and presented in this report.

Second, the TOC scoring framework is tiered to reflect the level of local compliance. Jurisdictions may be rated as fully compliant, substantially compliant, or partially compliant, with defined thresholds established for each policy area. Strict compliance with every standard is not required to remain eligible for future grant funding opportunities. However, higher levels of compliance improve overall scoring and strengthen a jurisdiction’s competitiveness in what are expected to be highly competitive funding processes.

For ease of reference, the eleven (11) standards and requirements, which stem from the four policy elements, are summarized below:

¹ Tier 3 is defined as having a stop or station served by one BART line, Caltrain, light rail transit, or bus rapid transit.

Policy Areas	Applicable Standard / Requirement
1. Residential Densities	50 du/ac average min.; 75 du/ac average max.
2. Commercial Office Intensities	2.0 FAR average min.; 4.0 FAR average max.
3. Housing Policies	2 production, 2 preservation and 2 protection
4. Commercial Stabilization	1 stabilization policy
5. Parking Standards	Min: Not allowed. Max: ≤1 res/unit; ≤2.5/1,000 sf commercial
6. Bicycle Parking Standards	Min: 1 secure bike res/unit; 1/5,000 sf commercial
7. Parking Management Policies	Unbundled, shared, plus 1 additional parking management policy
8. Complete Streets	Verify adopted policy
9. Active Transportation	Demonstrate local prioritization of an active transportation project serving station area.
10. Access Gap Analysis	Identify and plan for how to address access barriers to transit
11. Mobility Hub Planning	Plan/implement hubs with shared mobility & curb management.

To be deemed “fully compliant,” a jurisdiction must achieve a minimum score of 85 points, with a maximum possible score of 100 points. Points are allocated and weighted differently across policy areas (see related discussion under “Analysis”).

Recent Amendments to State Law:

In consideration of MTC’s TOC policies, it is important to consider the impact of new bills and laws that touch on key policy areas. A summary of more recently adopted legislation, and their overlap with identified policy areas, is provided as follows:

- [**SB 79 \(2025\)**](#) (Overlap with Policy Area #1) – Increases permitted residential density, height, and floor area ratio around transit-stops.
 - Within ¼ mile of Hamilton, Campbell, or Winchester Stations:
 - Height: Up to 65-feet
 - Density: Up to 120 units per acre
 - Residential FAR: Up to 3.0
 - Between ½ and ¼ mile of Hamilton, Campbell, or Winchester Stations:
 - Height: 55-feet
 - Density: Up to 80 units per acre
 - Residential FAR: Up to 2.5; bonus for projects ≥60 units/acre.
 - Projects immediately adjacent to TOD stops are eligible for:
 - Height: +20 feet in height
 - Density: + 30 dwelling units per acre

- Residential FAR: +1 residential FAR

Allows the City to adopt a Transit-Oriented Development Alternative Plan (TOD-AP) to reassign and allocate densities within specified areas around transit-stops, provided that the same total capacity is achieved and between 50-200% of the initial density is maintained. SB 79 is effective July 1, 2026 (reference Attachment 2 – SB 79 Summary).

- [AB 87 \(2025\)](#) (Overlap with Policy Area #2) – Caps the permitted commercial floor area ratio that may be granted as part of a density bonus project to two and a half times (2.5x) the floor area ratio otherwise allowed.
- [AB 130 \(2025\)](#) (Overlap with Policy Area #6) – Limits the City’s ability to adopt building reach codes that are stricter than state minimums for residential buildings, from October 1, 2025 through June 1, 2031, unless one of several narrow exceptions (i.e., required to implement General Plan policy) are satisfied.
- [AB 894 \(2023\)](#) (Overlap with Policy Areas #5 & #7) – Requires shared parking to count toward meeting minimum parking requirements when meeting specified requirements (i.e., existing parking is determined to be underutilized²).
- [AB 1317 \(2023\)](#) (Overlap with Policy Area #7) – Requires unbundled parking in new residential developments with 16 or more units that are issued a certificated of occupancy after January 1, 2025.
- [AB 2097 \(2022\)](#) (Overlap with Policy Area #7) – Eliminated minimum parking standards within one-half mile of public transit.
- [AB 413 \(2023\)](#) (Overlap with Policy Area #7) – Prohibits parking within 20 feet of any marked or unmarked crosswalk.
- [AB 3177 \(2024\)](#) (Overlap with Policy Area #7 and #8) – Limited the City’s ability to require developers to dedicate right-of-way to widen roads in front of new housing developments to achieve a desired roadway width.
- [SB 960 \(2024\)](#) (Overlap with Policy Area #8) – Established new legal expectations for state routes or state-maintained roadways to include bike and pedestrian improvements during maintenance and repaving.

ANALYSIS

This section summarizes the key elements of the Transit-Oriented Communities (TOC) policies, evaluates the City’s current level of compliance, and outlines staff’s recommended actions to strengthen alignment where appropriate. Each policy standard is presented with a summary of the requirements, followed by staff’s assessment of existing conditions and identification of potential next steps.

² Underutilized parking” means parking where 20 percent or more of a development’s parking spaces are not occupied during the period that the parking is proposed to be shared by another user, group, development, or the public.

1. Average Residential Densities

Policy Standard: Residential density policies are evaluated based on the degree of compliance with the minimum and maximum permitted densities across all station areas within a jurisdiction. The TOC Policy establishes three tiers for achieving compliance, with corresponding points, that improve competitiveness for future grant funding, as follows:

Standard	Policy Compliance Thresholds		
	Full (8 Points)	Substantial (6 Points)	Partial (4 Points)
Min. Average Density	50 du./ac.	43 du./ac.	38 du./ac.
Max. Average Density	75 du./ac.	64 du./ac.	56 du./ac.

When calculating average densities, the following areas are counted by the methodology:

- Parcels, or those portions thereof, that are located within the station area boundary as determined by MTC.
- Areas located in zoning districts that permit common forms of residential development (i.e., excludes open space, industrial, and zones that only permit less typical residential uses such transitional housing).
- Excludes parcels with dwelling units that received a building permit prior to January 1, 2024³.

Staff Analysis: The City of Campbell establishes permitted residential density ranges (minimum and maximum) based on land use designations, as shown on the [General Plan Land Use Map](#). As part of the City’s adopted Housing Element, new Transit-Oriented Mixed-Use (TO-MU) designations were intentionally established to encourage and support higher-density residential development near the City’s transit stations. The densities permitted under these zoning districts are consistent with the density expectations set forth in the Metropolitan Transportation Commission’s (MTC) Transit-Oriented Communities (TOC) Policy.

However, it is important to note that the City did not apply the TO-MU designation to all parcels located within a one-half mile radius of its transit stations. Instead, this designation was selectively applied to identified Housing Opportunity Sites that were specifically evaluated and included in the Housing Element Sites Inventory. This targeted approach explains the gap between the City’s TOD housing strategy and full conformance with the broader geographic expectations of the MTC TOC Policy.

The following table captures the minimum and maximum average residential density across all station areas (as required by the policy), as well as on a station-by-station basis, to visualize the City’s compliance with the policy.

³ Provided that they were not counted as a housing opportunity site assumed to produce one or more housing units.

Station Area	Minimum Average Density (Thresholds: 50/43/38)	Maximum Average Density (Thresholds: 75/64/56)	
		Existing	Under SB 79
All Station Areas	38.03 du./ac. <i>(Compliant under lowest threshold)</i>	49.97 du./ac. <i>(Not Compliant)</i>	75 du./ac. <i>(Compliant under highest threshold)</i>
Hamilton Station	40.84 du./ac.	53.81 du./ac.	
Campbell Station	35.17 du./ac.	46.17 du./ac.	
Winchester Station	36.93 du./ac.	48.42 du./ac.	

As shown in the table above, the City satisfies the minimum compliance threshold for minimum average density (38.03 vs. 38), qualifying for up to 4 points under the TOC Policy. To achieve the maximum possible points in this category, the City would need to increase the minimum average density in station areas by approximately 12 dwelling units per acre. This increase could be realized either by raising the average density evenly across all three station areas or by strategically distributing any increase by individual station area(s).

Although the City does not currently qualify for points under the maximum average density standard, under SB 79 it is expected to reach a maximum average density of 75, which would yield 8 additional points in this category.

- **Recommended Action:** With the adoption of SB 79, allowable residential densities around transit stations will increase automatically, prompting the need to reevaluate how these intensities are distributed within the City’s transit-oriented areas. Rather than pursuing incremental adjustments to the City’s minimum and maximum residential densities at this time, staff recommends deferring such changes until next year to allow for a more comprehensive and strategic planning process. This approach would enable the City to thoughtfully direct higher-density development to areas that best support transit use while protecting locations where such intensities may conflict with existing community goals, such as preserving the historic scale and character of Campbell’s Downtown.

At that time, the City could evaluate potential adjustments to residential densities consistent with the Metropolitan Transportation Commission’s Transit-Oriented Communities (TOC) Policy, in conjunction with the preparation of a Transit-Oriented Development Alternative Plan (TOD-AP) as authorized under SB 79 (see related discussion under “Recent Amendments to State Law”). This work effort would be conducted independently from the Hamilton Avenue Public Improvement Plan, which has a narrower infrastructure-focused scope.

The following table summarizes the total points anticipated based on the staff recommendation.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommendation
1. Average Residential Densities			
Min. Average Density	8	4	4
Max. Average Density	8	0	8
Total Points	16	4	12

2. Average Commercial Office Intensities

Policy Standard: Like the residential density policy standards, average commercial office intensities are evaluated based on the degree of compliance with the minimum and maximum permitted floor area ratio (FAR) across all station areas within the City. The TOC Policy establishes three tiers for achieving compliance, with corresponding points that improve competitiveness for future grant funding, as follows:

	Policy Compliance Thresholds		
Standard	Full (4 Points)	Substantial (3 Points)	Partial (2 Points)
Min. Average Floor Area Ratio	2.0	1.7	1.5
Standard	Full (5 Points)	Substantial (4 Points)	Partial (3 Points)
Max. Average Floor Area Ratio	4.0	3.4	3.0

When calculating average commercial office intensities, the TOC Policy allows for areas that do not allow office as a principally permitted use to be excluded (such as non-commercial and industrial areas). For Campbell, staff exercised the City’s option to exclude the following areas:

- Properties with a Light Industrial or Research and Development land use designation (where office is a discretionary use); excepting the General Commercial/Light Industrial land use designation.

Staff Analysis: The vast majority of commercial office intensities are regulated by the Campbell Municipal Code’s development standards for zoning districts that support office uses, including the General Commercial, Office, and Neighborhood Commercial zoning districts ([Section 21.10.050](#)). These standards apply to both stand-alone commercial developments and the commercial components of mixed-use projects, with the residential portion excluded from the FAR calculation.

The development standards contained in [Section 21.10.050](#) establish a maximum floor area ratio (FAR) of 1.0 for most zoning districts that support office consistent with the land use restrictions established by the [2040 General Plan](#). This standard applies Citywide, except in the following areas:

- (1) the Pruneyard/Creekside Commercial District, where parcels three (3) acres or larger may be developed at a maximum FAR of 2.0;

- (2) the Winchester Boulevard Master Plan area, where properties are limited to a maximum FAR of 1.5; and
- (3) the CB-MU zoning district, where a base FAR of 1.25 is permitted and a maximum FAR of 1.5 may be granted by the Planning Commission or City Council.

While the City does not prescribe a minimum FAR for commercial or office development, it retains discretion over changes to building area when presented as part of a commercial or office project. In practice, projects generally increase the floor area of projects with projects proposing to reduce floor area below existing conditions being relatively uncommon.

The following table shows the minimum and maximum average commercial office intensities across all station areas (as required by the policy), as well as on a station-by-station basis, to visualize the City’s compliance with the policy.

Station Area	Minimum Average FAR (Thresholds: 2.0/1.7/1.5)	Maximum Average FAR (Thresholds: 4/3.4/3.0)
All Station Areas	N/A <i>(Not Compliant)</i>	1.32 <i>(Not Compliant)</i>
Hamilton Station	N/A	1.23
Campbell Station	N/A	1.42
Winchester Station	N/A	1.29

As shown in the table above, Campbell would need to establish a minimum FAR for office uses near transit stations that minimally would need to exceed the City’s existing average maximum permitted FAR (1.32 to 1.5). Additionally, the City would need to more than double the maximum permitted FAR (from 1.32 to 3.0) for commercial office uses near transit stations to score any points in this category.

Given that SB 79 will substantially increase allowable FAR for housing in transit areas, it may be appropriate for the City to revisit allowable commercial FAR to maintain parity between residential and employment-generating uses. Without such adjustments, housing projects could outpace the development potential of commercial properties, affecting the City’s jobs–housing balance and broader General Plan goals for economic vitality.

Revisiting allowable FARs in transit-served and mixed-use areas to bring them closer to industry standards—and potentially to the thresholds established under the TOC Policy—could improve project feasibility, attract reinvestment, and help maintain alignment with state housing law and regional planning objectives.

- Recommended Action: Similar to the approach to residential densities, staff recommends deferring any further consideration of changes to the City’s minimum and/or maximum floor area ratio standards until next year. At that time, the City could evaluate potential adjustments to commercial office intensities in conjunction with the preparation of a Transit-Oriented Development Alternative Plan (TOD-

AP), as provided for under SB 79 (see related discussion under “Recent Amendments to State Law”). This work effort would be conducted independently from the Hamilton Avenue Public Improvement Plan which has a narrower focus.

However, because SB 79 does not establish a minimum average FAR and would yield a maximum average FAR below what is needed to satisfy the TOC Policy, it remains too uncertain for staff to assume compliance. Accordingly, scoring in this category reflects the full range of possible points rather than assuming a likely outcome.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
2. Average Commercial Office Intensities			
Min. Average Floor Area Ratio	4	0	0 to 4 (Unknown)
Max. Average Floor Area Ratio	5	0	0 to 5 (Unknown)
Total Points	9	0	0 to 9 (Unknown)

3. Housing Policies

Policy Standard: To fully comply with the Affordable Housing requirements, the City must adopt at least two (2) policies for each of the housing topic areas: Production, Preservation, and Protection. The TOC Policy includes a list of eligible policies for each of the three topic areas and establishes the regulatory requirements for policy.

The City believes that it has made significant progress to meet the threshold requirements for Production and Protection Policies and proposes to meet the Preservation Policies as described below.

- **Production Policies**

The City has implemented five (5) of the seven (7) Production Policies and has likely met the threshold requirements with the following production policies.

- Production Policy 1: Inclusionary Zoning
- Production Policy 2: Affordable Housing Funding
- Production Policy 3: Affordable Housing Overlay Zones
- Production Policy 4: Public Land for Affordable Housing
- Production Policy 5: Ministerial Approval

- **Preservation Policies**

The City has not met the threshold requirement for Preservation Policies and proposes to meet them by implementing the following two eligible programs.

- Preservation Policy 4: Condominium Conversion Restrictions
- Preservation Policy 8: Preventing Displacement from Substandard Conditions

- **Protection Policies**

The City has likely met the threshold requirements of Protection Policies due to current implementation of the following two (2) protection policies:

- Protection Policy 3: Legal Assistance for Tenants
- Protection Policy 10: Fair Housing Enforcement

Standard	Policy Compliance Thresholds	
	Full (8 Points)	Partial Compliance (4 Points)
Production Policies	2+ adopted policies	1 adopted policy
Preservation Policies	2+ adopted policies	1 adopted policy
Protection Policies	2+ adopted policies	1 adopted policy

Staff Analysis: The Transit-Oriented Communities (TOC) Policy requires jurisdictions to adopt at least two policies under each of the three affordable housing topic areas—Production, Preservation, and Protection—to demonstrate compliance.

Production Policies

The City of Campbell has implemented five (5) of the seven (7) eligible Production Policies, meeting the TOC Policy’s threshold for this category. Existing local programs and ordinances establish a strong foundation for affordable housing production, including:

- Production Policy 1: Inclusionary Zoning – Adoption of Chapter 21.24 (Inclusionary Housing) of the Campbell Municipal Code (CMC), which lowered the applicability threshold from 10 to 5 units and added an in-lieu fee option.
- Production Policy 2: Affordable Housing Funding – Establishment of a local housing trust fund, inclusionary fee revenues, and commitment of \$2.5 million in new affordable housing funds through pending development agreements.
- Production Policy 3: Affordable Housing Overlay Zone – Zoning incentives that facilitate affordable housing on underutilized sites.
- Production Policy 4: Public Land for Affordable Housing – Ongoing evaluation of surplus or underutilized City-owned property for potential affordable housing use.
- Production Policy 5: Ministerial Approval – Implementation of streamlined review processes consistent with state housing law.

These initiatives, together with the City’s recent Prohousing Designation from the California Department of Housing and Community Development (HCD), collectively demonstrate substantial compliance with the TOC Policy’s Production requirements.

Standard	Implemented Housing Policies	
	Required Policies (Thresholds: 2 adopted/1 adopted)	Implemented Policies
Production Policies	# of adopted policies	5 (Compliant)

Preservation Policies

The City has not yet met the TOC Policy’s minimum threshold for Preservation Policies. The City previously pursued a Transit-Oriented Communities (TOC) Housing Policies Grant from the Metropolitan Transportation Commission (MTC) to support development of the two Preservation Policies identified below but was not awarded funding. The City now proposes to follow through with this work through adoption of the following eligible programs:

- Preservation Policy 4: Condominium Conversion Restrictions – To protect existing Below Market Rate (BMR) rental units from conversion, the City intends to adopt regulations requiring 1-for-1 replacement or equivalent in-lieu fees, tenant right-of-first-purchase, and relocation assistance.
- Preservation Policy 8: Preventing Displacement from Substandard Conditions – To prevent loss of affordable housing due to substandard conditions, the City proposes to strengthen its housing rehabilitation and code enforcement programs to ensure safe, habitable units for low-income tenants.

Adoption of these two Preservation Policies would bring the City into full compliance with the TOC Policy requirements for this category.

Implemented Housing Policies			
Standard	Required Policies (Thresholds: 2 adopted/1 adopted)	Implemented Policies	Proposed Policies
Preservation Policies	# of adopted policies	0	2 (Condo Conversion & Preventing Displacement)

Protection Policies

The City currently meets the TOC Policy’s Protection Policy threshold through existing tenant protection and fair housing programs:

- Protection Policy 3: Legal Assistance for Tenants – Provided by Project Sentinel under contract with the City.
- Protection Policy 10: Fair Housing Enforcement – Ongoing City-funded partnership with Project Sentinel for discrimination investigation and enforcement services.

Together, these policies provide essential protections for renters and align with the TOC Policy’s intent to ensure housing stability.

Implemented Housing Policies		
Standard	Required Policies (Thresholds: 2 adopted/1 adopted)	Implemented Policies
Protection Policies	# of adopted policies	2 (Compliant)

- **Recommended Action:** Establishing the two Preservation policies will bring the City into compliance with the “3Ps” requirement aligning it with the goals of the Plan Bay Area 2050 and improving its competitiveness for MTC discretionary funding. The Condo Conversion Restriction is supported by recent trends in the Campbell community, while the Preventing Displacement policy is supported by Program H-2f in the [City’s 2023-2031 Housing Element](#). Given their citywide scope, these policies should be advanced through a separate work plan and added to the City Council’s FY 2026–27 budget priorities.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
3. Housing Policies			
Production Policies	8	8	8 (No action required)
Preservation Policies	8	0	8 (Condo Conversion and Preventing Displacement)
Protection Policies	8	8	8 (No action required)
Total Points	24	16	24

4. Commercial Stabilization Policies

Policy Standard: To comply with the commercial stabilization requirement, the City must adopt at least one policy to support small businesses in TOC areas. This standard is intended to help mitigate potential displacement of existing businesses as redevelopment occurs near qualifying stations.

The policy options are:

1. Small Business/Nonprofit Overlay Zone (1 Point): Create a new zoning overlay with defined boundaries, benefits and eligibility criteria.
2. Small Business/Nonprofit Preference Policy (1 Point): Require property owners to provide a “right of first offer” for small businesses and nonprofits to locate within new projects.
3. Small Business/Nonprofit Financial Assistance (1 Point): Provide direct financial assistance to qualifying businesses and nonprofits in the TOC areas with dedicated funding required for at least four years.
4. Small Business Advocate Office (1 Point): Designate a staff resource as a single point of contact to connect small businesses with technical assistance and support services.

For scoring purposes, no points are awarded in this category for partial compliance, and a maximum of one (1) point, regardless of the number of policy options implemented, may be awarded for this category.

Staff Analysis: The City has not yet met the TOC Policy’s minimum threshold for Commercial Stabilization Policies, which requires adoption of at least one policy to support small businesses within TOC areas.

Small businesses are central to Campbell’s economy. According to City business license data, of Campbell’s 2,827 businesses, nearly three-quarters employ fewer than 15 workers, and firms with fewer than 50 employees account for more than half of all employment. Given this profile, Campbell faces exposure to business displacement pressures as properties redevelop. The TOC Policy anticipates this challenge and requires jurisdictions to adopt at least one stabilization measure to apply in TOC areas or citywide.

Staff recommend pursuing Option 4 (Small Business Advocate Office) at this time. The tools contemplated in the first three options (overlay zones, preference policies, and financial assistance) would require new regulatory structures, sustained funding commitments, or administrative monitoring requirements beyond current capacity. Most of these tools do not address the specific drivers of displacement in Campbell, which stem from the high cost of tenant improvements and limited availability of suitable relocation sites. For example, a “right of return” provision does little to assist a displaced tenant unable to finance build-out costs years later, or an industrial tenant that cannot relocate into the type of commercial/retail space replacing its former site.

Designating a small business advocate function, by contrast, can be implemented within existing resources and builds on the City’s established practice of providing outreach, referrals and general business support. The City’s Economic Development Manager, housed in the City Manager’s Office, already serves as the primary point of contact for businesses, connecting them to City programs, regional partners, and technical assistance resources. To formalize this role for TOC compliance, staff will update the City’s website to clearly identify the Economic Development Manager as small business advocate. While this role provides meaningful support, it is important to recognize that not all business displacements can be mitigated as the General Plan and future transit-oriented projects are built out.

Commercial Stabilization		
Required Policies	Implemented Policies	Proposed Policies
One (1) adopted policy to support small businesses in TOC areas	0	1 (Small Business Advocate Office)

Recommended Action: Recognize the City’s existing Economic Development Manager role as fulfilling the small business advocate option under MTC’s TOC Policy. Updating the website can be completed within the calendar year and will not result in significant staff costs, additional workload, or adoption of new local policies.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
4. Commercial Stabilization			
Adopt at least one policy for commercial stabilization	1	0	1 (Small Business Advocate Office)
Total Points	1	0	1

5. Parking Standards

Policy Standard: The TOC Policy establishes both minimum and maximum parking standards for areas determined by the jurisdiction to be subject to [Gov. Code Section 65863.2](#) (hereinafter “AB 2097”, 2022) (rather than using the station areas as the basis of analysis) as follows:

	Policy Compliance Thresholds		
Minimum Parking Standard	No Parking Requirement (2 Points Each)		
Residential	No minimum parking requirement allowed (policy also <i>encourages</i> jurisdictions to forgo any exemption process from AB 2097 ~ not applicable to Campbell).		
Commercial			
Maximum Parking Standard	Full (6 Points)	Substantial (4 Points)	Partial (2 Points)
Residential (per unit)	0.5	0.6	0.7
Commercial ⁴ (per 1,000 sq. ft.)	1.6	1.9	2.1

Staff Analysis: TOC Policy and AB 2097 (see related discussion “Recent Amendments to State Law”) have similar provisions with respect to parking minimums. Broadly speaking, both TOC Policy and AB 2097 prohibit a public agency from imposing or enforcing parking minimums on any residential, commercial, or other development project located within a ½-mile of a major transit stop. The City demonstrates compliance with state law, and TOC Policy, by eliminating minimum parking requirements within ½ mile of a major transit stop, as provided by [Section 21.28.045.A](#) (Reductions in the number of parking spaces required) of the Campbell Municipal Code. Therefore, the City is anticipated to comply with this TOC standard.

Regarding parking maximums, the City does not have a maximum parking standard for any land use. If the City were to adopt a maximum parking standard, developers would be prohibited from proposing parking above the adopted threshold (i.e., establishing a “cap”) [within those same areas covered by AB 2097](#).

- Housing Projects: For new housing developments, projects utilizing State Density Bonus Law would likely be able to exceed or waive maximum parking limits through the use of concessions, incentives, or waivers.

For all other residential projects, strict application of maximum parking standards could have unintended consequences. In particular, single-family or small-scale infill projects within AB 2097 areas would be prohibited from providing onsite parking, while larger multi-unit developments would be compelled to rely on shared parking facilities.

⁴ The TOC Parking Standards Policy only applies to office, retail, and service type business types.

As noted in the City’s prior correspondence to the Metropolitan Transportation Commission (MTC) during adoption of the Transit-Oriented Communities (TOC) Policy, imposing maximum parking standards in Campbell could have a chilling effect on both residential and commercial development. Feedback from both market-rate and affordable housing developers indicates that most new projects in Campbell—even those located within one-half mile of transit—continue to provide onsite parking to meet resident expectations and to satisfy financing and lender requirements. Over time, as higher-density housing becomes more prevalent and regional transit service improves, these market dynamics may shift and reduce the need for onsite parking.

Given current conditions, staff does not recommend adoption of maximum parking standards at this time.

- **Commercial Projects:** For new commercial development projects, ample customer parking remains a key site selection criterion for tenants, particularly in suburban markets. Many tenants will not even consider a location if they do not see a path to meeting their standard parking criteria. Imposing a parking cap would limit the pool of potential tenants and discourage businesses the City may wish to attract, making Campbell less competitive with neighboring jurisdictions that allow higher parking ratios. This directly influences the City’s fiscal health, given that retail businesses contribute to local sales tax revenues. The issue is particularly important to Campbell, given the City’s relatively thin sales-tax base and small inventory of strong commercial development sites.

The following table presents typical parking ratios for various land uses, along with example brands commonly associated with each category.

Use Type	Example Business	Average Measured Demand (Spaces / 1k sq. ft. ⁵)
Superstore / Discount Retail	Walmart, Target	2.73
Warehouse Club	Costco, Sam’s Club	3.26
Liquor / Specialty Beverage	BevMo!, Total Wine & More	2.60
Supermarket / Grocery Store	Safeway, Trader Joe’s	6.90
Home Improvement	Home Depot, Lowe’s	4.09
Pharmacy / Drugstore	CVS, Walgreens	3.58
Big-Box Electronics / Appliances	Best Buy, Fry’s (defunct)	1.93
Furniture / Large-Item Retail	IKEA, Ashley Furniture	3.45 ⁶
General Retail (Mid-Box)	Ross, Marshalls, PetSmart	2.56
Small Retail / Strip Center	Multi-tenant plaza	5.45
Medical Office	Carbon Health, Western Dental, MinuteClinic	4.28

Source: ITE Parking Generation Manual, 6th Edition (2023)

⁵ 85th percentile rate unless otherwise noted.

⁶ 85th percentile rate unavailable; entry shows average rate.

If the City were to comply with TOC parking standards, it would need to adopt maximum parking limits that are lower than many of its existing minimum requirements. For instance, the TOC Policy would cap residential parking at 0.7 spaces per unit (regardless of size), while the City’s lowest residential requirement is 0.5 space per unit (for single-room occupancy units only). Similarly, the City’s lowest commercial parking requirement, 2.5 spaces per 1,000 square feet for warehouse uses, would exceed the TOC maximum, which would limit commercial parking to 2.1 spaces per 1,000 square feet of floor area.

Areas	Min. Parking (Threshold: 0)	Maximum Parking	
		Residential (Thresholds: 0.5/ .6/ .7)	Commercial (Thresholds: 1.6/1.9/2.1)
Within ½ Mile of Major Transit (Gov. Code Section 65863.2)	0 <i>(Compliant)</i>	N/A <i>(Not Compliant)</i>	N/A <i>(Not Compliant)</i>
Outside ½ Mile of Major Transit (Gov. Code Section 65863.2)	N/A - Reference Only	<i>For Reference Only</i> Minimum Parking Standards <u>Single-Room Occ.</u> .5 spaces / unit <u>Walkable Areas</u> ⁷ : 1 to 2 spaces / unit (depending on # of bedrooms & unit size) <u>Non-Walkable Areas</u> : 1 to 2½ spaces/unit (depending on # of bedrooms & unit size)	<i>For Reference Only</i> Minimum Parking Standards <u>Commercial Uses; Typ.</u> 2.5 – Warehouse ⁸ 2.8 – Financial 4 – Service Uses 4.4 – Professional Office 5 – Medical / Retail ⁹ Varies – Other Uses

- **Recommended Action(s):** Considering the City’s recent adoption of lower residential parking standards ([Ordinance No. 2293](#), May 2, 2023), which sought to balance the City’s historic requirements with those allowed under State Density Bonus Law, staff does not recommend further changes to the residential parking standards at this time.

For commercial uses, given the need to remain competitive in attracting high-quality commercial businesses, staff does not recommend adopting the TOC parking standards.

⁷ Many walkable areas overlap with areas within ½ mile of public transit.

⁸ The policy only applies to office, retail, and service businesses; warehouse is included for reference.

⁹ But not less than 2 spaces per use, plus 1 space for each 1,000 sq. ft. of outdoor display area.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
5. Parking Standards			
Minimum Residential Parking	2	2	2
Minimum Commercial Parking	2	2	2
Maximum Residential Parking	6	0	0
Maximum Commercial Parking	6	0	0
Total Points	16	4	4

6. Bicycle Parking Standards

Policy Standards: Off-street bicycle parking standard thresholds for new residential or commercial development, and associated point scores, are summarized as follows:

Standard	Policy Compliance Thresholds	
	Full (3 Points)	Partial Compliance (2 Points)
Residential:	1 secure bike space(s) per dwelling unit	0.85 to 0.99 secure bike space(s) per dwelling unit
Commercial:	1 secure bike space(s) per 5,000 sq. ft.	0.85 to 0.99 secure bike space(s) per 5,000 sq. ft.

Staff Analysis: The City’s short and long-term bicycle parking requirements have been established in compliance with [California Green Building Standards Code](#) which outline the following minimum bicycle parking obligations:

- Short-term: 5% of vehicle parking spaces for businesses anticipated to generate visitor traffic, with a minimum of a two-bike capacity rack.
- Long-term: 5% of vehicle parking spaces for buildings with 10 or more tenant occupants, with a minimum of one bicycle parking facility.

In circumstances where a use may not provide any parking (i.e., in areas covered by AB 2097), or may otherwise be eligible for parking standards below these minimum thresholds, the City’s Parking and Loading Ordinance establishes a separate [minimum bicycle parking requirement](#) of one parking space per four residential dwelling units, or 2,500 sq. ft. of non-residential gross floor area. However, the City does not establish a requirement for secure bicycle parking spaces for either residential or commercial uses.

The table below compares the City’s Bicycle Parking Standards with the TOC Standard:

Areas	Minimum Bicycle Parking	
	Residential (Thresholds:1 per unit) (3 Points)	Commercial (Threshold: 1 per 5,000 sq. ft.) (3 Points)
All Station Areas	~.25 per unit; not secure ¹⁰ <i>(Not Compliant)</i>	1 per 2,500 sq. ft.; not secure <i>(Not Compliant)</i>

- **Recommended Action:** Given that the City of Campbell may be limited in adopting bicycle parking standards that exceed state minimums for residential buildings (see “Recent Amendments to State Law”; AB 130), staff recommends postponing any changes to the residential bicycle parking standards. Consideration should resume only after confirmation from MTC or the City Attorney’s Office that such changes would be legally permissible. In light of AB 130, MTC may revise the standard or allow alternative compliance options for jurisdictions that did not adopt a standard prior to September 30, 2025—the deadline for incorporating such changes into a local reach code. Depending on the outcome, anywhere from 0 to 3 points may be awarded in this category.

For commercial uses, staff would be open to requiring one secure bicycle parking space per 5,000 square feet of new commercial building area, as an alternative to providing two unsecure spaces, if it would qualify for additional point in this category. However, implementing such a requirement would necessitate further analysis of potential costs and consultation with the business community to confirm that a greater number of open bike spaces (as the current code requires) would not be more heavily used and preferred in most contexts. However, since this effort would extend beyond the boundaries of the Hamilton Avenue Public Improvement Plan, staff recommends that the City Council plan for this work to be considered enough other possible work plan items for FY 2026–27.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
6. Bicycle Parking Standards			
Residential Bicycle Parking	3	0	0 to 3 (Unknown)
Commercial Bicycle Parking	3	0	3
Total Points	6	0	3

7. Parking Management Policies

Policy Standard: Jurisdictions must:

- Explicitly state that unbundled¹¹ parking is allowed by code.
- Explicitly state that shared parking is allowed between uses by code.

¹⁰ Exception: Housing projects located in “walkable areas” as indicated on the [Form-Based Zone Map](#) are required to provide bicycle parking spaces in an indoor/secure area ([Section 3.020.6.A.](#)).

¹¹ Unbundling parking means separating the cost of leasing a parking space from the sale or rental price of residential and commercial uses

- Adopt at least one complementary parking management strategy from the MTC/ABAG Parking Policy Playbook such as: Transportation Demand Management (TDM) policy, curb strategy, parking benefit district, demand-responsive pricing, or priced parking.

For scoring purposes, no points are awarded in this category for partial compliance. A maximum of one (1) point may be awarded for each fully compliant policy.

Policy Standard	Points Possible
Unbundled Parking	1
Shared Parking Between Land Uses	1
Complementary Parking Management Strategies	1
Total Possible Points	3

Staff Analysis: The following discussion is organized into three (3) focus areas covering unbundled parking, shared parking between uses, and consideration of complementary parking management strategies.

- **Unbundled Parking:** While the City currently allows unbundled parking, a minor amendment to the Municipal Code would be needed to explicitly clarify that “unbundled parking” is permitted to ensure compliance with the TOC Policy.

Additional amendments may also be necessary to remove existing requirements to bundle (assign) parking to qualify for parking reductions in shared parking scenarios (see related policy discussion below).

Unbundled Parking		
Required Policies	Implemented Policies	Proposed
Explicitly state unbundled parking is allowed by code.	0	Update code to explicitly state unbundled parking is allowed.

- **Shared Parking Between Land Uses:** While the City allows shared parking among similar land use categories (e.g., residential or non-residential), the Municipal Code generally prohibits shared parking between dissimilar uses. In other words, commercial uses (such as retail, restaurant, or medical office) may share parking with each other, and residential uses (such as apartments or condominiums) may do the same, but not across categories.

A limited exception exists for mixed-use projects, where up to 50% of required guest parking may be credited toward the non-residential parking requirement, provided the project meets applicable standards related to parking management, location, and signage.

Separately, the City recognizes the necessity to update Chapter 21.28 (Parking and Loading) of the Campbell Municipal Code to allow for shared use of underutilized parking as provided for by AB 894 (see related discussion under “Recent Amendments to State Law”) and plans to address these changes as part of a comprehensive code update scheduled for review and adoption in 2026.

Shared Parking		
Required Policies	Implemented Policy	Proposed
Allow shared parking between land uses.	N/A	Update code to allow shared parking between land uses in transit areas.

- Complementary Parking Management Strategies:** The City currently requires new businesses with 50 or more employees during morning peak hours to implement Transportation Demand Management (TDM) measures—such as showers, carpool/vanpool parking, and on-site amenities—to reduce traffic and improve air quality.

Existing light rail stations already incorporate curb strategies: Hamilton Station includes a kiss-and-ride zone on Creekside Way, Downtown Campbell Station provides a drop-off lot on Railway Avenue, and Winchester Station offers park-and-ride facilities.

Shared Parking	
Required Policies	Implemented Policies
Adopt at least one complementary parking management strategy.	1 (TDM Program)

- Recommended Action:** Staff would recommend clarifying the City’s policies related to unbundling and allowing for shared parking between land uses consistent with the TOC Policy.

With respect to adopting at least one complementary parking management strategy, staff recommends taking no further action at this time, as Campbell’s existing Transportation Demand Management (TDM) Strategy appears to meet the intent of this requirement.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
7. Parking Policies			
Unbundled Parking	1	0	1
Shared Parking Between Land Uses	1	0	1
Complementary Parking Management Strategies	1	1	1 (No action required)
Total Points	3	1	3

8. Complete Streets Policy

Policy Standard: Jurisdictions must:

- Adopt a compliant complete streets policy.

For scoring purposes, no points are awarded in this category for partial compliance. A total of three (3) points may be awarded for adoption of a compliant Complete Streets policy.

Staff Analysis: Adopted in 2013, the City of Campbell’s Complete Streets Policy establishes policies and principles consistent with the California Complete Streets Act of 2008 and aligns with the TOC Policy.

Complete Streets Policy	
Required Policies	Implemented Policies
Adopt a compliant complete streets policy	1

- Recommended Action: Because the City has adopted a Complete Streets Policy consistent with the TOC Policy, no further action is needed for the City to receive the maximum possible points in this category.

Policy Area	Points Possible	Current Points	Points Anticipated
8. Complete Streets Policy			
Adopt a compliance complete streets policy.	3	3	3 (no action required)
Total Points	3	3	3

9. Prioritization of Active Transportation Projects

Policy Standard: To demonstrate that the City has prioritized implementation of active transportation projects within the TOC area, a jurisdiction must submit at least one (1) of the following:

- A relevant “All Ages and Abilities” (AAA) active transportation project identified in the City’s current Capital Improvement Program (CIP) with.
- A relevant “AAA” active transportation project funded or submitted for funding (e.g., OBAG, ATP, etc.) within the past five years.
- Other funding or implementation plans that include an AAA relevant project.

Staff Analysis: Regarding compliance with MTC’s Active Transportation Policy, the City has consistently prioritized projects that enhance walking and biking access within and around the TOC area. City staff actively seeks opportunities to incorporate “All Ages and Abilities” bicycle and pedestrian improvements into Capital Improvement Program (CIP) projects. Recent examples include the installation of bike lanes as part of the Winchester Boulevard Resurfacing Project in 2021, completion of the Pruneyard Creek Trail earlier this month, and the buffered bike lanes to be constructed with the upcoming Hamilton Avenue Resurfacing Project.

Within the past five years, the City has advanced several significant active transportation initiatives supported by regional funding. The design of the Campbell Priority Development Area (PDA) Enhancements Project—funded through OBAG and completed in 2024—provides enhanced pedestrian and bicycle infrastructure along Civic Center Drive and Orchard City Drive. Construction of Phase 1, focused on Civic Center Drive and Second and Third Streets, has been completed. In addition, the Hamilton Avenue/State Route 17 Bicycle Overcrossing Feasibility Study, funded by Measure B, is currently underway and anticipated to be completed early next year.

Active Transportation Projects	
Requirement	Representative Projects
At least one relevant AAA active transportation project in CIP, funded, or implemented within and around the TOC area.	<i>Winchester Boulevard Resurfacing Project, Pruneyard Creek Trail, Hamilton Avenue Resurfacing Projects, Campbell Priority Development Area Enhancements Project, etc.</i>

- Recommended Action: Based on the City’s demonstrated progress in implementing and funding “All Ages and Abilities” active transportation projects within and around the TOC area, no further action is required for the City to meet the Active Transportation Policy standard and receive full points in this category.

Policy Area	Points Possible	Current Points	Points Anticipated
9. Active Transportation			
Prioritize Active Transportation Projects	7	7	7 (no action required)
Total Points	7	7	7

10. Access Gap Analysis

Policy Standard: Identify three (3) adopted capital or service improvements that would expand the area around each station that can be accessed:

1. On foot within 10 minutes;
2. By active transportation (e.g., bike, scooter, or similar) within 15 minutes; and
3. By bus within 15 minutes.

Jurisdictions may select these three improvements from one or more of the following adopted documents:

- The circulation element of a locally adopted PDA, specific, precise, or area plan.
- A transit agency–adopted station access plan.
- An adopted active transportation, bicycle, or pedestrian plan.
- The circulation element of the General Plan.

For scoring purposes, points are not awarded in this category for partial compliance. Each access improvement type (pedestrian, active transportation and bus) is worth 3 points. A total of nine (9) points may be awarded in this category.

Policy Standard	Points Possible
Pedestrian	3
Active Transportation (Bike)	3
Bus	3
Total Points	9

Staff Analysis: The following projects and plans include pedestrian and bicycle improvements that fulfill two (2) of the three (3) Access Gap requirements.

2018 Countywide Bicycle Plan

Regional plan adopted by the City identifying key bicycle connections, including a future overcrossing at Hamilton Avenue and State Route 17.

FY2026–2030 Capital Improvement Plan (CIP)

The City of Campbell’s CIP includes supporting projects as follows:

- Accessible Pedestrian Signals (APS) Project: Citywide ADA-compliant signal upgrades (beginning FY27).
- Transit Signal Priority (TSP) Expansion Project: Signal modernization along major VTA corridors (target completion Winter 2027).

Transportation Improvement Plan for the Campbell PDA (“TIPC Study”)

Downtown Campbell plan guiding phased pedestrian and bicycle improvements and developing a multimodal corridor concept along Campbell Avenue.

Multimodal Transportation Plan (MMTP)

Citywide plan to build a connected, equitable transportation network and guide future mobility investments.

General Plan Transportation Element

The City’s General Plan Transportation Element demonstrates consistent policy alignment with MTC’s station access objectives. Key provisions include:

Goal T-1: *Create a Complete Streets network that accommodates all users.*

Policy T-1.1: Develop and implement a connected multi-modal transportation network that balances transportation modes, encourages non-automobile travel, and reduces greenhouse gas emissions, while promoting healthier travel alternatives for all users and respecting context.

Action T-1.h: Improve pedestrian and bicycle access to bus and light rail stations when evaluating opportunities with new development proposals and capital improvement projects. In cooperation with VTA, evaluate transit-waiting environments to improve convenience and comfort.

Action T-1.h aligns with VTA's Station Access Policy (adopted on 10/12/18) which includes the following strategy.

- Identify first/last mile barriers to transit including infrastructure needs (e.g., bike parking, sidewalks, crosswalks), connecting services (e.g., shuttles, bike share), and wayfinding and transit information gaps.

These provisions demonstrate clear policy alignment with the Access Gap Analysis standard and ensure that near-term projects advance within a consistent long-range vision for equitable, multimodal access throughout the community.

Integration of AHSC Program Opportunities

The State's [Affordable Housing and Sustainable Communities \(AHSC\) Program](#), administered by [HCD](#), funds projects that reduce greenhouse gas emissions through affordable housing and transportation improvements such as active transportation, complete streets, and better transit access.

Within the TOC Policy area, AHSC grants could help:

- Deliver bicycle and pedestrian improvements identified in the City's MMTP, CIP, Hamilton Avenue Public Improvement Plan, and [Winchester Station Access Study](#);
- Upgrade bus stops, crosswalks, lighting, and wayfinding near VTA stations; and
- Support affordable housing connected to enhanced mobility options.

These investments can advance TOC Policy goals while leveraging State and regional funds rather than direct City dollars.

- Recommended Action(s): While a formal access gap analysis and consolidated station access improvement program have not yet been completed for the Hamilton Avenue TOC area, the City's adopted and ongoing planning efforts—including the VTA Countywide Bicycle Plan, Hamilton Avenue corridor projects, MMTP, and General Plan Transportation Element—collectively advance the intent of MTC's TOC access policy.

Staff recommends continuing to monitor and pursue AHSC and similar State funding opportunities to implement eligible multimodal improvements and address access deficiencies identified through ongoing planning efforts. If additional regional analysis is requested by MTC or partner agencies, staff recommends such work be coordinated at the regional level, given the cross-jurisdictional nature of the transit network.

At this time, staff does not recommend advancing this effort in the FY26–27 budget cycle without a dedicated funding source but will pursue strategic partnerships and AHSC opportunities to support equitable station access and multimodal investments.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
10. Access Gap Analysis			
Pedestrian	3	3	3
Active Transportation (Bike)	3	3	3
Bus	3	0	0
Total Points	9	6	6

11. Mobility Hub Planning & Implementation

Policy Standard: Mobility Hubs support first and last mile connections through access to multiple travel options. MTC’s objective for implementing mobility hubs is to create community anchors. This includes achieving consistent and coordinate mobility, creating exceptional experience, reducing GHG emissions, leveraging development and mobility partnership opportunities, achieving equitable spaces, and creating safe environment around mobility hubs.

This policy requires jurisdictions to identify one project or enhancement that supports the creation of a community anchor at each station. Enhancements may include (but not limited to):

- Safety improvements
- Bike parking
- Electric charging infrastructure (bikes, scooters, carshare)
- Public realm improvements (e.g., lighting, green infrastructure)
- Information improvements (e.g., wayfinding, real-time information)
- Any other active transportation access improvements within the TOC area

Jurisdictions may either; (1) Identify such a project or enhancement in an existing plan or project; or (2) demonstrate prioritization of such an enhancement with a current or prior funding application for MTC’s Mobility Hub Program for the TOC area.

Staff Analysis: MTC’s [Mobility Hub Playbook](#) promotes a flexible “kit-of-parts” approach tailored to local conditions. The Hamilton Avenue Light Rail Station is positioned to serve as an Emerging Urban District Hub, supported by surrounding land uses and planned

improvements identified in the Hamilton Avenue Public Improvement Plan (PIP). Near-term opportunities include enhanced wayfinding, lighting, and bicycle parking, with longer-term potential for electric charging and green infrastructure.

Staff have initiated coordination with VTA to define partner roles and ensure improvements, some of which may occur outside the public right-of-way, are planned collaboratively. The City’s experience from the Hamilton Avenue PIP, including corridor walk audits and multimodal access mapping, will inform similar planning at the City’s other light rail stations to achieve TOC Policy consistency.

Current and planned projects that advance Mobility Hub objectives include:

- **Hamilton Avenue Resurfacing Project:** Adds buffered bike lanes and green lane markings to improve first- and last-mile access (completion expected summer 2026).
- **Hamilton Avenue Public Improvement Plan Walk Audit:** Identifies pedestrian access and safety enhancements.
- **Existing Station Amenities:** Hamilton Station includes a kiss-and-ride area and e-bike lockers; additional wayfinding improvements are planned.

These initiatives demonstrate the City’s ongoing efforts to improve multimodal access and align with the intent of MTC’s Mobility Hub policy.

- Recommended Action: Acknowledging that the Hamilton Avenue Public Improvement Plan is expected to guide discussions on satisfying mobility hub planning principles, staff recommends waiting for the plan’s adoption before estimating potential points in this policy area or determining whether the identified approaches should be applied to the City’s two remaining station areas.

Although this process is intended to support TOC Policy compliance and enhance the City’s scoring, the City’s ultimate ability to achieve compliance remains uncertain.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
11. Mobility Hub Planning etc.			
Adopt a complaint mobility hub plan/project.	6	0	0 to 6 Unknown
Total Points	6	0	0 to 6

ACTION SUMMARY

With the incorporation of staff’s recommended actions, the City is anticipated to achieve greater alignment and enhance the City’s competitiveness for future One Bay Area Grant (OBAG 4) funding opportunities. The following table summarizes anticipated score by category with recommended actions reflected.

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
1. Mobility Hub Planning etc.			
Min. Average Density	8	4	4
Max. Average Density	8	0	8
2. Average Commercial Office Intensities			
Min. Average Floor Area Ratio	4	0	0 to 4 (Unknown)
Max. Average Floor Area Ratio	5	0	0 to 5 (Unknown)
3. Housing Policies			
Production Policies	8	8	8
Preservation Policies	8	0	8
Protection Policies	8	8	8
4. Commercial Stabilization			
Adopt at least one policy for commercial stabilization	1	0	1
5. Parking Standards			
Minimum Residential Parking	2	2	2
Minimum Commercial Parking	2	2	2
Maximum Residential Parking	6	0	0
Maximum Commercial Parking	6	0	0
6. Bicycle Parking Standards			
Residential Bicycle Parking	3	0	0 to 3 (Unknown)
Commercial Bicycle Parking	3	0	3
7. Parking Policies			
Unbundled Parking	1	0	1
Shared Parking Between Land Uses	1	0	1
Complementary Parking Management Strategies	1	1	1
8. Complete Streets, Active etc.			
Compliant Complete Streets Policy	3	3	3

Policy Area	Points Possible	Current Points	Points Anticipated with Recommended Action
9. Active Transportation			
Prioritize Active Transportation Projects	7	7	7
10. Access Gap Analysis			
Pedestrian	3	3	3
Active Transportation (Bike)	3	3	3
Bus	3	0	0
11. Mobility Hub Planning etc.			
Adopt a complaint mobility hub plan/project.	6	0	0 to 6
Total Points	100	41	63 to 81

Based on an anticipated score of 63 to 81 points, assuming completion of staff recommended actions (see summary below), the City of Campbell is likely to be found to be “partially compliant” with MTC’s initial draft TOC scoring framework¹² (developed in the spring of 2025).

Staff recommended actions, including those which will have an unknown outcome, are summarized as follows:

- Policy Area 2 (Average Commercial Office Intensities) – Adopt higher Average Commercial Office Intensities (FAR).
- Policy Area 3 (Housing Policies) – Adopt housing preservation policies for condominium conversion and preventing displacement.
- Policy Area 4 (Commercial Stabilization) – Establish a small business advocate office.
- Policy Area 6 (Bicycle Parking Standards) – Establish secure bicycle parking requirements for residential and commercial uses.
- Policy Area 7 (Parking Policies) – Update the code to allow for unbundled parking and shared parking between land uses.
- Policy Area 11 (Mobility Hub Planning) – Adopt a compliant mobility hub plan.

Transit-Oriented Community Policy Scoring Brackets		
Not Compliant	Partially Compliant	Fully Compliant
0 – 39	40 – 84 (63 to 81 Points Anticipated)	85+

Jurisdictions in this scoring range remain eligible for certain regional funding programs, such as OBAG, and can apply for planning grants. However, staff notes that a revised

¹² MTC anticipates finalizing the TOC Policy framework and scoring criteria in early 2026.

TOC scoring framework is anticipated to be adopted by MTC at the end of October 2025. The revised framework is expected to establish the relationship between TOC score and OBAG funding eligibility, for the OBAG 4 cycle and potentially for future OBAG cycles.

Should the City want to go further, as to be found “fully compliant” (scoring 85+ points) with the TOC Policy, one or more of the following actions not recommended by staff, would need to be considered:

- Policy Area 1 (Average Residential Density) (+4 Points) – Increase average minimum required density from 50 du/acre to score maximum points in this category (only 4 points currently anticipated).
- Policy Area 5 (Parking Standards) (+12 Points) – Establish maximum residential and/or commercial parking standards.
- Policy Area 10 (Access Gap Analysis) (+3 Points) – Demonstrate that the City has adopted capital or service improvements that would expand the area around each station that can be accessed by bus service. (Note: Satisfying this policy by itself would not be enough; $81+3= 84$ Points).

ENVIRONMENTAL DETERMINATION

This Study Session is not a “project” under CEQA (CEQA Guidelines §15378) as it involves policy review only. CEQA review will occur with adoption of the Hamilton Avenue Public Improvement Plan and/or as part of future planning efforts as described in the staff analysis and/or potential future actions.

FISCAL IMPACT

As noted throughout this report, staff-recommended actions will require additional review and fiscal impact analysis as part of the FY 2026–27 budget process.

Some actions, such as potential adoption of a Transit-Oriented Development Alternative Plan (TOD-AP), may advance ahead of this schedule for consideration in Spring 2026. Preparation of a TOD-AP would likely require consultant support and could involve costs that well exceed \$100,000.

PUBLIC OUTREACH

Courtesy email notifications were sent to subscribers of the City’s email service to provide updates on major planning efforts. The update was also posted on the Hamilton Public Improvement Plan project website and the City’s agenda center. As of the date this report was prepared, no public feedback had been received.

Additionally, outreach to MTC and VTA was conducted to support the development of this report and the City’s planned policies and actions.

PREPARED BY: Stephen Rose, Senior Planner
Matthew Jue, Traffic Engineer
Roger Storz, Senior Civil Engineer

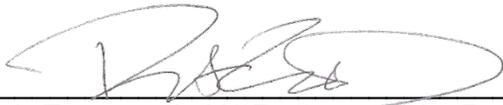
Eloiza Murillo-Garcia, Housing Manager

Nathan Donato-Weinstein, Economic Development Manager

Report Approval Details

Document Title:	Study Session on Transit-Oriented Communities (TOC) Policy
Attachments:	Attachment 1 – City Feedback on TOC Scoring Framework Attachment 2 – SB 79 Summary
Final Approval Date:	October 23, 2025

This document and all of its attachments were approved and signed as outlined below:

Approved by: 
Rob Eastwood, AICP, Community Development Director



CITY OF CAMPBELL
Community Development Department

via electronic mail

May 5, 2025

Eli Kaplan
Regional Planning Program
Metropolitan Transportation Commission
375 Beale Street
San Francisco, CA 94105
ekaplan@bayareametro.gov

RE: Campbell's Comments on the Draft MTC TOC Policy Scoring Framework

Regional Planner Eli Kaplan:

The City of Campbell appreciates the opportunity to provide comments on the draft Transit-Oriented Communities (TOC) Evaluation Criteria Framework understanding its role in OBAG 4 funding eligibility. We are fully supportive of the TOC goals focusing on transit-oriented development to meet the broader sustainability needs of the region. However, we are significantly concerned that the TOC Scoring Framework prescribes an approach that does not consider existing and evolving market conditions and in certain areas will act to discourage the feasibility of developing transit-oriented development projects. Please accept our feedback on key aspects of the framework based on the unique situations anticipated to be faced by local jurisdictions, including the City of Campbell, when applying for points under the draft scoring framework.

Draft MTC TOC Policy Scoring Overview

The Draft MTC TOC scoring framework requires jurisdictions to meet prescribed standards across four core areas—land use density, affordable housing (production, preservation, protection), parking management, and transit station access.

These requirements are scored using a 100-point framework, equally weighted across the four categories, to determine whether a jurisdiction is classified as “Fully Compliant,” “Partially Compliant,” or “Not Compliant,” with corresponding implications for OBAG 4 funding eligibility. The standards are intended to support equitable transit-oriented development near fixed-guideway transit corridors.

Campbell Feedback

The City of Campbell has recently adopted a General Plan and Housing Element that focuses on transit-oriented development, by planning for and supporting high density residential development around our three light rail stations. As such we strongly support the regional vision of fostering equitable, transit-supportive communities and aligning local efforts with broader state objectives. However, the current TOC evaluation framework introduces prescriptive requirements that may not be feasible or effective for all jurisdictions, particularly smaller, built-out cities like Campbell, given local constraints, economic conditions, and the recent completion of several significant long-range planning efforts.

Key concerns with the draft framework include:

- **Residential Density Requirements:** The framework's heavy emphasis on residential density does not reflect current market conditions, where high-density housing is often financially infeasible due to high construction costs, labor shortages, and limited investor interest. Many jurisdictions, including Campbell, recently updated their Housing Element and/or General Plan and adopted rezonings through extensive public engagement to satisfy RHNA 6 requirements. Additional density increases at this time would duplicate recent work and may undermine the integrity of those planning processes.

The relevance of awarding TOC points for residential density is also reduced by the current legislative environment. State laws, including AB 2011, SB 6, SB 35, SB 330, SB 423, SB 450, SB 1123, and updates to Density Bonus Law, increasingly preempt local standards and allow for higher densities regardless of local land use designations, particularly for affordable or mixed-income housing projects. The TOC scoring methodology does not appear to fully reflect these legal changes, resulting in a disconnect between the framework and the regulatory context in which jurisdictions are operating.

Parking Standards: A large share of the parking management category's 25 points is tied to adopting maximum parking standards. Eliminating parking minimums and unbundling parking from housing costs are already mandated by AB 2097 and other recent state laws. The low maximum ratios (e.g., 0.375 spaces/unit) under the TOC policy are likely unrealistic in TOC areas with limited transit coverage or constrained last-mile access. Note that under State Density Bonus Law (Gov. Code § 65915), developers may waive parking maximums that would physically preclude the construction of affordable housing. This limits the enforceability of such standards.

As a broader point, the City has received extensive feedback from residential developers that they will not pursue higher density transit-oriented development projects if the city is to impose maximum parking standards, based on customer demand and lending requirements. Combined with the current challenges already

facing the higher density housing market (interest rates, softer rental market, construction costs), the TOC mandate of parking maximums will likely have the opposite effect of stifling the feasibility to develop high density, transit-oriented projects, countering the goals of MTC's TOC program. MTC is encouraged to reduce the weight assigned to parking caps or allow flexibility through locally supported market studies and feasibility analyses.

Additionally, TOC points should reflect aspirational local efforts beyond state mandates, such as establishing transportation demand management policies, requirements for bicycle storage lockers, and adopting strategies to improve curb space management and equitable access to shared mobility, rather than duplicating compliance obligations.

- Office FAR and Commercial Stabilization Policies: Requirements to adopt minimum office FARs or commercial stabilization policies fail to reflect post-pandemic realities. Widespread remote work, persistent commercial vacancies, and evolving land use trends continue to reshape downtowns and station areas. Jurisdictions should be permitted to propose data-supported alternatives, such as adaptive reuse strategies or economic development incentives to more effectively utilize existing space, in place of fixed office density targets.
- Station Access and Averaging Across TOC Areas: The framework calls for robust planning and capital investment in Complete Streets, active transportation networks, access gap analyses, and mobility hubs. While the City fully supports these objectives, these expectations may be difficult to meet in smaller jurisdictions with limited staff or resources – particularly in the near term. Moreover, averaging TOC scores across multiple station areas can dilute strong progress in one area due to conditions in another, particularly in cities like Campbell which are behind in policy implementation efforts and have a higher-than-average number of stations per capita.
- Alignment with HCD's Prohousing Designation Program: Several TOC policy requirements overlap with strategies recognized in the State's Prohousing Designation Program administered by HCD, which incentivizes housing production through streamlined funding and regulatory benefits. The Housing Element certification process already evaluates development feasibility in detail, including factors like lot size, parcel readiness, and zoning capacity. MTC is strongly encouraged to align the TOC framework with the Prohousing scoring methodology, and to credit jurisdictions that have adopted RHNA-compliant zoning, exceeded their RHNA allocations, or implemented other HCD-validated strategies to facilitate housing near transit. This would ensure consistency between regional and state housing objectives and prevent duplicative or contradictory compliance efforts.
- Set-aside Incentive for Top Tier Jurisdictions: An MTC staff slide presentation at the April 9 VTA Technical Advisory Committee (TAC) mentioned a "Set-aside incentive for the top tier" (i.e., fully compliant jurisdictions). This particular slide

showed a bell-shaped curve but did not indicate what amount or percentage of OBAG 4 funds would be set aside. We are concerned that no jurisdictions in Santa Clara County will qualify for the fully compliant tier. Santa Clara County is the most populous county in the Bay Area. To set aside funds for a handful of top tier jurisdictions works against geographic equity.

To ensure the TOC Policy is a realistic and equitable tool for all jurisdictions, Campbell recommends the following refinements:

- **Phase-In Compliance:** Implement a phased scoring approach that awards full credit based on increasing compliance over time. For example:
 - By 2026: One-third of policy areas compliant to receive full credit
 - By 2027: Two-thirds compliant to receive full credit
 - By 2028: Full compliance across policy areas to receive full credit
- **Allow Per-Station Scoring:** Jurisdictions should be allowed to earn full points, and qualify for funding, for individual TOC areas that meet all applicable criteria, even if other areas within the city are still in progress.
- **Rebalance Scoring Weights:** Reduce emphasis on criteria that are legally preempted (e.g., parking caps, density requirements), economically infeasible (e.g., office FAR, parking maximum standards), or duplicative of state mandates (e.g., AB 2097). Increase weighting for local strategies that align with HCD's Prohousing Designation program or demonstrate meaningful advancement of Plan Bay Area 2050 goals.
- **Support Legal and Technical Implementation:** If MTC retains standards vulnerable to legal override (e.g., through Density Bonus Law concessions or waivers), the agency should provide model findings, technical documentation, or regional legal support to assist jurisdictions in defending or implementing these standards.
- **Award OBAG 4 Funds Based on Project's Merit:** Award of OBAG 4 funds should be based on the merits of a project. Use a scoring system that awards points on a tiered scale based on the level of TOC policy compliance. Do not preemptively set aside OBAG 4 funds.

We look forward to continued engagement with MTC as the TOC Policy implementation process moves forward. Please feel free to contact me at (408) 866-2141 or by email at robe@campbellca.gov if any clarification is needed regarding our comments.

Sincerely,

A handwritten signature in black ink, appearing to read 'Rob Eastwood', enclosed within a large, loopy oval shape.

Rob Eastwood, AICP
Community Development Director

cc: Brian Loventhal, City Manager
Amy Olay, Interim Public Works Director
Eloiza Murillo-Garcia, Housing Manager
Stephen Rose, Senior Planner
Matthew Jue, Traffic Engineer
Sophie Gabel-Scheinbaum; via email at sgabel-scheinbaum@bayareametro.gov

SB 79 (Wiener) –Transit-Oriented Development: Housing Density Incentives

Upzones housing within ½ mile of “Tier 1” and “Tier 2” transit stops in “urban transit counties.”

Eligibility Criteria



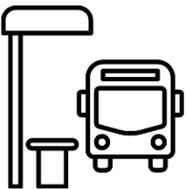
- **Projects must include:** ≥5 units
- **Minimum density:** The greater of any local density minimum or 30 units/acre
- **Zoning:** Any property "zoned for residential, mixed, or commercial development" eligible.
- **Exemptions:** Certain requirements do not apply to cities with population <35,000

Baseline Development Standards



- **Average Unit Size:** ≤1,750 net habitable sq. ft.
- **Incentives:** Affordable projects meeting min. densities qualify for density bonus incentives
- **Inclusionary Requirements (for projects > 10 units):**
 - The greater of any local requirement or: 7% ELI | 10% VLI | 13% LI

Tiered Transit Stop Standards



- **Tier 1 (Heavy Rail / High Frequency Commuter Rail):**
 - Height: 65-75 feet | Density: 100-120 units/acre | FAR 3.0-3.5
- **Tier 2 (Light Rail / High Frequency Bus / Commuter Rail):**
 - Height: 55-65 feet | Density: 80-120 units/acre | FAR 2.5-3.0
- **Projects immediately adjacent to TOD stops are eligible for:**
 - +20 feet in height | +40 dwelling units per acre | + 1 residential FAR

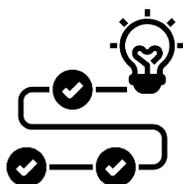
Enforcement & Labor Requirements



- **Effective Date:** Operative on July 1, 2026, or at 7th RHNA cycle for unincorporated areas
- **Transit Agencies:** Allows transit agencies to create their own TOD projects on their own land near transit stops; can supersede zoning for the site
- **Enforcement:** HCD given enforcement authority
- **Labor Standards:** Labor standards required for projects > 85' and transit agency TODs

Alternative TOD Plans

Allows jurisdictions to adopt a Transit-Oriented Development Alternative Plan (TOD-AP) to reassign and allocate densities within specified areas around transit-stops, provided the same total capacity is achieved.



- **Must adopt as:** Housing Element amendments, specific plans, overlays, or ordinances
- Must keep overall development capacity (units & floor area) | 50%–200% of required capacity
- Exceptions (until 1 year after 7th Cycle RHNA):
 - VHFHSZ | Sea level rise areas (1 ft) | Historic sites (state/national register) | Sites with ≥50% of SB 79 capacity

Disclaimer: This document provides a general overview of SB 79 and is not intended to serve as legal advice or a legal reference. For complete details, please refer to the official statute.